



The Switchlist

Official Publication of the Pacific Northwest Region, National Model Railroad Association

February 2024

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Form 255
UNION PACIFIC RAILROAD COMPANY

SWITCH ENGINEMEN'S DAILY REGISTER

Yard _____ 19 ____

Highest Bulletin Number	Left Round House or Store Track	SHIFT WORKED		Arrived Round House or Store Track	HOURS WORKED			Accumulated Miles	Explanation of Meal Period, Overtime, Delays, etc.
		Began Work	Quit Work		Meal Period	Straight Time	Over Time		

Illinois Central Gulf

Switchlist

YRS15, Friday, February 7, 1982 at: Reserve
 7:15 AM Throttle: 0 Next Yard: St. Rose, LA
 Drew Madore Origin: Reserve, LA
 7-027 Destination: St. Rose, LA
 UP 2701 Cabir: 199044

be	SPOT	NxtLoc	Tons	Contents	Hdg
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Hall 2 42 700M 4865

Form 818-A Standard—Small
**SWITCH CARS CAREFULLY AND SAFELY
 AVOID ROUGH HANDLING**
 Santa Fe
 (Insert Name of Railway Company)

At _____ Station, 1-3-82

Train No. 350/p Engine No. 470/p Time 6/p M.

CAR	Initials	No.	Contents	Destination	Consignee	Tonnage
1						
2		40	Williams Bros			
3		44	68262			
4		27	68758			
5						
6						
7						





The Switchlist

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The Switchlist is the official publication of the Pacific Northwest Region, National Model Railroad Association, and is published quarterly in February, May, August, and November.

SUBSCRIPTIONS

The Switchlist is available free of charge online at the PNR website, pnr.nmra.org. PNR members with current email addresses on file with the NMRA will receive an email notice when the latest issue is posted.

CONTRIBUTIONS

Readers of *The Switchlist* are encouraged to submit news, articles, photos, and drawings about PNR activities past, present and future. Articles on other aspects of model railroading are also appreciated.

To assist you with article preparation, the PNR website has posted a set of [guidelines](#) for formatting and submitting articles, photos, and other material for publication in *The Switchlist*. This also includes a style guide and list of key terms that should be helpful.

The Editor reserves the right to accept, reject, and edit contributions as space, time, and suitability of material dictate. The *Switchlist* does not pay for articles.

Please email your contributions to the editor at switchlist@pnr.nmra.org. Deadlines for submissions are:

February 1
May 1
August 1
November 1

Complete staff and contact information is available in this newsletter and on the PNR website: pnr.nmra.org.

ADVERTISING

Non-commercial classified advertising will be accepted from PNR members at no charge as a member service. Email submissions to the Editor. Hobby stores that support model railroading may also include an advertisement. See the section titled "PNR Interchange" for more details.

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The Fourth and Fifth Dimensions

Here's a rhetorical question ... If time and energy (among other factors) are needed to build a model railroad, and if *time* is considered by some to be the fourth dimension, would *energy* be the fifth dimension? Before you ask, yes, I'm old enough to remember that the Fifth Dimension was a pop vocal group from the late 1960s, but I'm looking beyond that.

Anyway, I'll leave you to ponder that one. In this edition of *The Switchlist*, we have another update on the [Surrey Excursion](#). Check out the number of registrants listed on p.3! We're doing great, and it's definitely not too late to join in the fun! So, the convention is a situation where you can apply some of your time and energy. Check your calendar, formulate your travel plans, go to the website, and register. It's that easy!

Several other opportunities/needs for time and energy have emerged (p.5). These include finding nominees for PNR president and vice president and nominees for the 2024 PNR President's Award. We also continue our search for someone to serve as Education Committee Chair. And a new, critical need has arisen. Our long-serving PNR webmaster Mark Johnson has had to step down due to health issues. So, we are desperate to find a replacement. Mark: Thank you so much for the many years of service to the Region! We wish the very best for you in the years to come.

Second Division Superintendent Jeff Shultz has graciously volunteered to handle the PNR website management, but he can do this only in the short term. So anyone with some basic skills in website maintenance is desperately needed and should contact President Jeff Herrmann.

On page 6, President Jeff briefly describes the transition the Region has made to using Constant Contact for communicating with our membership. Until recently, one of our key members had been paying for this service out of his own pocket. Thank you, sir, for subsidizing us up to this point!

One more item about the [Surrey Excursion](#)—John Martin shares some thoughts about one of the many great clinics to be presented in Surrey.

Feature articles in this edition include: a visit to Cal Sexsmith's Parkland Division layout; Part 2 of MMR Bob Parrish's article on how to build AP quality structures; and Brian Orysen's approach to standardizing turnout controls for Free-Mo modules.

The *In Memoriam* section (p.7) lists those fellow PNR members who have recently passed away. Our sincere condolences go out to the family and friends of all these members. The PNR's newest members this quarter are listed on p.8. Hey all you veteran members: Be sure to review this list and reach out to those folks who may live near you. It's pretty easy—just contact your division superintendent to have them get you connected with these new members. Hmmm. A small investment in time and energy again and well worth it!

The PNR officer, staff, and division superintendent quarterly reports begin on p.21. And take note that the list of NMRA Partners has been updated with some new businesses added (p.29). The *Timetable* list of upcoming events on p.31 is beginning to fill out. But also check out the [NMRA Calendar](#) for events, especially if you'll be traveling outside of the PNR. One example is the Pacific Coast Region's 2024 convention in Silicon Valley: [The Tech Line](#).

Many thanks to our contributors to *The Switchlist*! Your time and energy are much appreciated. And I hope our readers get inspired to apply some time and energy to modeling and also assisting our members and organization.

As always, stay safe and happy modeling!

Greg Kujawa, Editor

PNR News and Announcements

Surrey Excursion PNR 2024 Convention!

Lori Sebelley, Convention Chair, 7th Division

The Surrey Excursion Convention will take place May 22-26, 2024, and be held at the Sheraton Guildford Hotel in Surrey, British Columbia.

We are now in the second stage of registration fees being \$149.00. This rate will expire midnight March 31. The registration rate will then be increased to \$169.00. Act quickly to take advantage of the second stage fee. Thank you to all who took advantage of the first stage and registered early.

Registration pricing gives access to self-guided layout tours, contests, displays, AP evaluation, clinics, operation sessions, and in-house non-rail activities.

All clinics, display/contest, banquet, and non-rail rooms will be located on the 2nd floor of the hotel. Access to all these rooms is by conveniently located elevators.

We have 50 clinic spots arranged for Thursday, Friday, and Saturday. Please take a look at the website as the topics and brief descriptions are being continually added under the Clinics tab. If you are interested in giving a clinic, please contact Marc Simpson @ surreyexcursionclinics@gmail.com.

There are still a handful of Deluxe King rooms available @ \$185+ taxes per night and Club rooms @ \$225+ taxes per night. Club rooms feature Club lounge access, private access to your floor, complimentary non-alcoholic beverages, honour bar, and complimentary business services. The Club lounge is open 24 hours. These prices are in **Canadian** dollars.

As of February 13, our Convention numbers are as follows:

- Full Fare Registrations – 151
- Non-Rail – 29
- Youth – 4
- Banquet – 116
- Modeling With The Masters (MWTM) – 13
- G3 Grain Terminal Tour – morning: 20, afternoon: 20
- Operations – 82
- Celebration Room – Achievement Program: 10, Contests: 17, Display: 16

A schedule has been posted on the website. Check out the convention website and please check back often for additional updates. <https://pnr2024.7divpnr.ca/>



See you in 2024 at Surrey Excursion!

PNR President and Vice President Election— Call for Nominations

Shirley Sample, Chair

It's that time again! 2024 is the year that we elect a new President and VP of the PNR. It is the job of the nominations committee to come up with at least two candidates for each position. Neither job takes a lot of time and most business is conducted via emails.

The committee consists of Robert Leatha, Brian Stokes, and me. Our email addresses are: rleatha38@gmail.com; bstokesndp@gmail.com, or shirley@busnws.com. My cell # is 509-991-2317 or home #509-292-8332.

If you are interested in being nominated or can think of someone who might be willing, please let one of the committee members know so that person can be contacted to confirm. Then we can get a candidate profile on them to be published in the next issue of *The Switchlist*. Nominations deadline is April 15th.

The Eligibility requirements and the Terms of Office are included below from the Bylaws.

Duties of BOD Members

1. The President shall:
 - a. Preside over meetings of the BOD and the Annual Membership Meeting.
 - b. Sign all contracts or other instruments in writing authorized by the BOD.
 - c. Be bonded in an amount sufficient to cover all PNR assets.
 - d. Call special meetings of the BOD whenever deemed necessary.
 - e. Have and exercise, under the direction of the BOD, the general administration of the affairs of PNR.
 - f. Be responsible for enforcing the Bylaws and Policies of PNR.
 - g. Appoint various officers and committees as needed with approval of the BOD.
 - h. Appoint various committee chairs without the advice and consent of the BOD.
2. The Vice President shall:
 - a. Perform the duties of the President during his or her absence.
 - b. Assist the President as requested.
 - c. Be responsible for membership promotion.

Eligibility

1. Each candidate for region office must:
 - a. Be a PNR member.
 - b. Be a natural person.
 - c. Not be under suspension from holding office.

Nominations

1. The procedures for nominations shall be published in the Region's official communication medium no later than 15 March of an election year.
2. Candidates for President and Vice President shall be nominated by one of the following methods:
 - a. Action of the Nominating Committee as published in the Region's official communication medium no later than 15 March.
 - b. A petition filed with the Nominating Committee Chairman no later than 15 April. Such a petition shall contain the signatures of at least 15 voting members of PNR and shall be accompanied by a written statement of consent signed by the candidate.
 - c. A letter of intent to return from incumbents running for re-election, or from a Vice President running for President. Letters of intent must be filed with the Nominating Committee Chairman no later than 15 April.

PNR President's Award—Call for Nominations

Dave Liesse, Chair

Nominations for the PNR President's Award for 2024 are now open. Any member of the Pacific Northwest Region may submit one or more nominations following the procedures listed below.

The President's Award honors a PNR member who has, through signal service to the hobby of model railroading, made the most significant contribution to the benefit and betterment of the PNR for at least the 12 months prior to the close of nominations.

This is not a popularity contest and nominations should be held in confidence. Send them directly to the committee chair, Dave Liesse, to be received no later than 31 March 2024. Please use one of the following addresses:

Email: nmradave@liessefamily.net

Regular mail:

Dave Liesse
c/o Skingco Services, Inc
33720 9th Ave S, Ste 9
Federal Way, WA 98003-6735

Nominations must include the following:

- Nominator's name and contact information
- Nominee's name and division

- Reasons for the nomination, preferably addressing one or more of these categories:
 - NMRA national service
 - PNR regional service
 - Divisional service
 - Club participation
 - Membership recruitment, retention, and service efforts
 - Public support of the hobby
 - Creative contribution to the NMRA, PNR, or division
 - Other significant contributions in support of the hobby
- Additional comments in support of the nomination

Previous winners may win the award again, but not if they've won in the last three years. Committee members, naturally enough, are also ineligible. The following are not eligible this year:

- Jeff Herrmann (PNR president)
- Richard Kurschner (PNR vice president)
- Dave Liesse (committee chair)
- Greg Kujawa (2023 winner)
- Lori Sebelley (2022 winner)
- Jack Hamilton (2021 winner)

For further information, please refer to PNR Policy M-2, found on page 24 of the PNR Policy Manual. The manual can be found on the PNR website at: pnr.nmra.org/policies.

Openings on the PNR Board of Directors

Jeff Herrmann, President

Two non-voting positions on the PNR Board of Directors have recently come open.

1. **PNR EDUCATION TEAM CHAIR:** Recruit and lead the PNR Education Team to:
 - Maintain and distribute to divisions a “catalogue” of quality clinics and educational materials that can be made available for on-line and in person meets and activities
 - Assist the host committee with clinic support for the PNR Annual Convention
 - Develop programs and teaching methods that appeal to new and younger modelers stressing: basic skills, innovation, current technology, tools, and procedures
 - Facilitate at least one approved “Modeling with the Masters” program in the region annually
 - Provide a natural educational path for members that introduces them to the Achievement Program (AP and MMR), contest involvement, and special interest groups (SIGs).
2. **PNR WEBMASTER:**
 - Maintain and manage the PNR website: <http://pnr.nmra.org/>
 - Ensure the functionality, security, and efficiency of the website infrastructure while optimizing loading speeds, fixing issues, and updating website content
 - Enhancing the user experience by creating a user-friendly, visually appealing, and secure site
 - Receive and post and/or update content as directed by the Board of Directors.

If you have interest in either of these positions or would like more information please contact:

Jeff Herrmann, President
Pacific Northwest Region, NMRA
PNRPREZ@nmra.org

Communicating With Our Members

Jeff Herrmann, President

At the November 2023 Board of Directors meeting, the board approved the creation of a new PNR Constant Contact contract to better communicate by email with all members of our region in an efficient manner. In addition to the region, each division will also be able to use the new contract for their own internal communication. Since this is a new contract, names from older PNR and Division mailing lists were not able to be migrated over to the new list.

Our new Constant Contact mailing list has been created from the current NMRA Membership Roster supplied by the national headquarters. Some folks who have “opted out” from receiving PNR and/or Division emails in prior years may have inadvertently been added back in because they are on our Membership Roster. We apologize in advance for this inconvenience.

If you would like to “opt out” from group emails from PNR or your home division, please notify your Division Superintendent or the PNR Office Manager, James Keena keenajames1@gmail.com.

An Eye-Catching Gem

John Martin, 7th Division

One thing many of us like about our hobby is that there is always something new to learn or discover. Have you ever heard of the “ALLCO?”

I’ve been watching as clinics are gradually being added to the PNR 2024 convention website at: pnr2024.7divpnr.ca or Surrey Excursion 2024.

Of the many great clinics being presented, one that really caught my eye is David Morgan’s “History of the Abernethy and Lougheed Logging Company” (ALLCO). In a word – WOW! I hadn’t even heard of this gem of a logging railroad and yet it was the largest logging operation in British Columbia at one point in the 1920s, and it included a busy fleet of Heisler and Climax geared steam locomotives.

David has a fascinating story to tell that includes aspects that still impact life here in the Lower Mainland of BC, including the Western Canada Power Company’s Stave Lake Reservoir, WW I contributions and post-war reorganization, and operations during the pre-Depression era heyday of coastal logging in the province. His presentation includes maps and photographs that will allow his audience to get a solid grasp of this unique and significant enterprise. It’s a riveting story, told by an experienced and entertaining presenter.

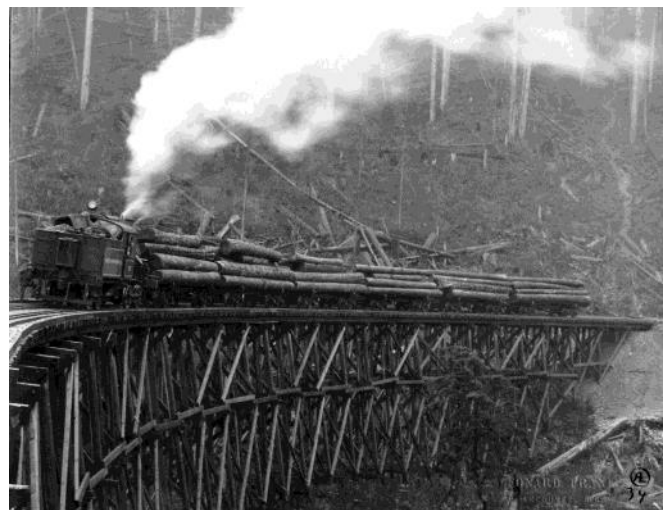
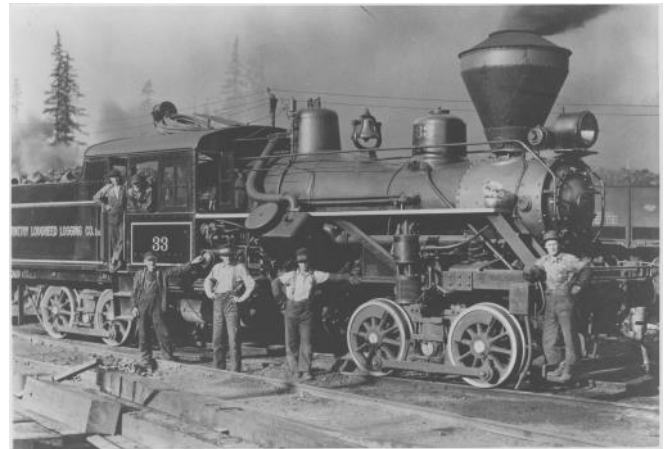
David has researched and written about the ALLCO and has spent hours walking the old railbeds with his faithful dog. He models parts of the operation in HO scale, and his projects include a beautifully constructed 7-ft trestle that accurately replicates the prototype, without compression.

His clinic is just one example of the high-quality presentations lined up for the 2024 Convention. If you haven’t registered yet, it’s easy to do at pnr2024.7divpnr.ca.

If you are one of the over 135 modelers who have already registered, may I suggest you visit the website and look at the updated material you’ll find there. The master schedule has been published, although we’re still firming up exact times and dates for specific clinics.

I’d also suggest that you give some consideration to booking an extra night or even two in the hotel at our significantly lower than current market rate. The month of May is historically a great time to visit the Vancouver area. You’ll find details under the website’s ACCOMMODATIONS tab.

We hope to see you there, especially at David Morgan’s clinic presentation. I think you’ll like it.



In Memoriam

This section is dedicated to the remembrance of those members of the NMRA Pacific Northwest Region whom we have lost over the past year. We extend our deepest sympathies, thoughts, and prayers to their families and friends. We will sincerely miss the fellowship of these folks and all their contributions to the NMRA and hobby.

Deceased Members of the PNR — January 2023 through January 2024

Month Reported	Div	Last Name	First Name	City/Town	State/Province	Special Recognition
January 2023	5	Lippincott	Linda	Newport	WA	Life Member
February 2023	7	Ackland	William D	Victoria	BC	
March 2023	5	Sala	John	Helena	MT	Life Member
	6	Warrington	Gary	Paradise Valley	AB	
April 2023	6	Lee	Anthony W	Lethbridge	AB	Life Member
	6	Hadlow	Bob	Calgary	AB	Life Member
May 2023	7	Heyer	Heinz	Sechelt	BC	
	5	Smith	Lauralyn	Hamilton	MT	
June 2023	4	Duvall	Duncan E	Port Townsend	WA	
	1	Selzer	Marvin	Coos Bay	OR	
July 2023	6	Seymour	Doug H	Edmonton	AB	Life Member
	4	Clarke	David L	Oak Harbor	WA	
August 2023	4	Johnston	J	Bellevue	WA	
	6	Golightly	James	Shaunavon	SK	
September 2023	7	Pate	Brian D	North Vancouver	BC	
	7	Sutcliffe	Richard A	Maple Ridge	BC	
October 2023	4	Ricketts	Charles W	Tumwater	WA	MMR #562
November 2023	1	Kang	Rick	Eugene	OR	Life Member
	4	Richards	Ray	Tacoma	WA	Life Member
December 2023	7	Legget	Robert	Coquitlam	BC	
January 2024	5	Yager	John	East Helena	MT	
	7	Klit	John	Trail	BC	

Have you changed your address or other membership information?

Notify NMRA Headquarters

email: hq@nmra.org

Phone: 423-892-2846 (8 a.m.–4 p.m. ET)

Mail: P.O. Box 1328, Soddy Daisy TN 37384-1328

**Not getting the bi-monthly
NMRA eBULLETIN?**

The new **NMRA eBulletin** comes out every other month near the first of the month. Either your SPAM filter is blocking it, or you need to update your email address with us. Have HQ update your address at: <http://www.nmra.org/customer-service-request>

New PNR Members

November 2023–January 2024

We extend a warm welcome to all new members of the Pacific Northwest Region, NMRA. Those listed below have joined the PNR over the past quarter. If you do not see your name listed, please be patient. Your membership is likely still being processed.

We are especially happy to welcome our new Rail Pass members ([identified in blue text](#)). We recognize that you are with us on a trial basis and we will have to work hard to convince you to stay with us. Please make sure to contact your Division Superintendent promptly (listing at the end of this newsletter) so you can be included in all Division activities.

Current Members: Please review this list for anyone living near you and consider initiating a connection with them through your Division Superintendent.

Div	Last Name	First Name	City/Town	State/Province	Div	Last Name	First Name	City/Town	State/Province
01	De Dios	Jesus	Klamath Falls	OR	05	Allaire	Charles	Moses Lake	WA
01	Marks	Bill	Reedsport	OR	05	Cambell	Daniel	Spokane	WA
01	Marks	Maureen	Reedsport	OR	05	Cates	Samuel	Spokane	WA
02	Adams	Donald	Lake Oswego	OR	05	Christensen	Tony	Pasco	WA
02	Brooks	William	Lake Oswego	OR	05	Christensen	Trey	Pasco	WA
03	Simpson	Nate	Meridian	ID	05	Christner	John	Hermiston	OR
03	Vargas	CJ	Meridian	ID	05	Zech	Bryson	Kennewick	WA
04	Butler	George	Bremerton	WA	05	Zintz	Dylan	Ellensburg	WA
04	Hartley	Scott	Poulsbo	WA	06	Creighton	Jonas	Camrose	AB
04	Khera	Paul	Juneau	AK	06	Dube	Danny	Hay River	NT
04	Kingsley	Michael	Arlington	WA	06	Factory	Little Toy	Leslieville	AB
04	Li	George	Newcastle	WA	06	Goian	Grady	Calgary	AB
04	Love	Robert	Lacey	WA	06	Page	Shayne	Edmonton	AB
04	Morris	Robert	Poulsbo	WA	07	Blanchette	Gus	Mission	BC
04	Muth	Sterling	Ft Wainwright	AK	07	Botting	Aidan	New Westminster	BC
04	Palms	Jim	Mountlake Ter	WA	07	Elter	Donald	Sechelt	BC
04	Patterson	Ed	Auburn	WA	07	Love	Dave	Kelowna	BC
04	Piccolo	Mike	Woodinville	WA	07	Mizuki	Junichi	Vancouver	BC
04	Schiefer	Jan	Seattle	WA					



PNR Constant Contact Resubscribe

Any member who has elected to unsubscribe to the PNR Constant Contact direct email may resubscribe, but Constant Contact only allows this to be done once for each member.

Here are the instructions to resubscribe:

1. Send an email to your division superintendent with your name, NMRA #, email address, and a request to resubscribe.
2. Division superintendents will send this information to the PNR Office Manager who will then send the resubscribe Constant Contact request to the member.

Features

Editor's Note: In the February 2022 Switchlist, we began running a series of articles featuring PNR members' layouts and modules. We've expanded the scope to now include club layouts. The intent of this series is to have an easy way to share your ideas, vision, modeling interests, and progress across the vast distances of the Pacific Northwest Region. These articles can be as simple or detailed as you want. You can review past issues of the newsletter to see what your fellow PNR members have submitted.

We've had a fairly good response so far but continue to need more articles to maintain momentum. Please consider submitting an article about your home or club layout or module.

The Parkland Division

Article, track plan, and photos by Cal Sexsmith, 6th Division

The Prototype

Although known as a prairie province, about half of Saskatchewan is forested. The transition zone between the prairie and the forest is known as the parklands. This zone, which is about 100 miles wide, is a mixture of open prairie and forest. The northern edge of the parkland is often called the forest fringe.

For the most part, railways did not extend north of the forest fringe. From a modelling point of view, this is an interesting area; in addition to the agricultural industries found further to the south, there is also an active forestry industry.

Prince Albert, which is located just south of the forest fringe, is Saskatchewan's third largest city and was the base of operations for both Canadian National and Canadian Pacific in the region. Interestingly, the CPR entered Prince Albert via trackage rights over the CNR. Figure 1 is a map of the prototype and Figure 2 is the trackage in Prince Albert.

In the 1960s, both railways originated several way freights and mixed trains on the various branch lines radiating out of the city. CNR's Duck Lake Subdivision and CPR's Prince Albert Subdivision served as the connection between Prince Albert and the rest of the world. Both subdivisions hosted speed freights and the Duck Lake Subdivision had conventional passenger trains into the 1960s and RDCs until 1981.

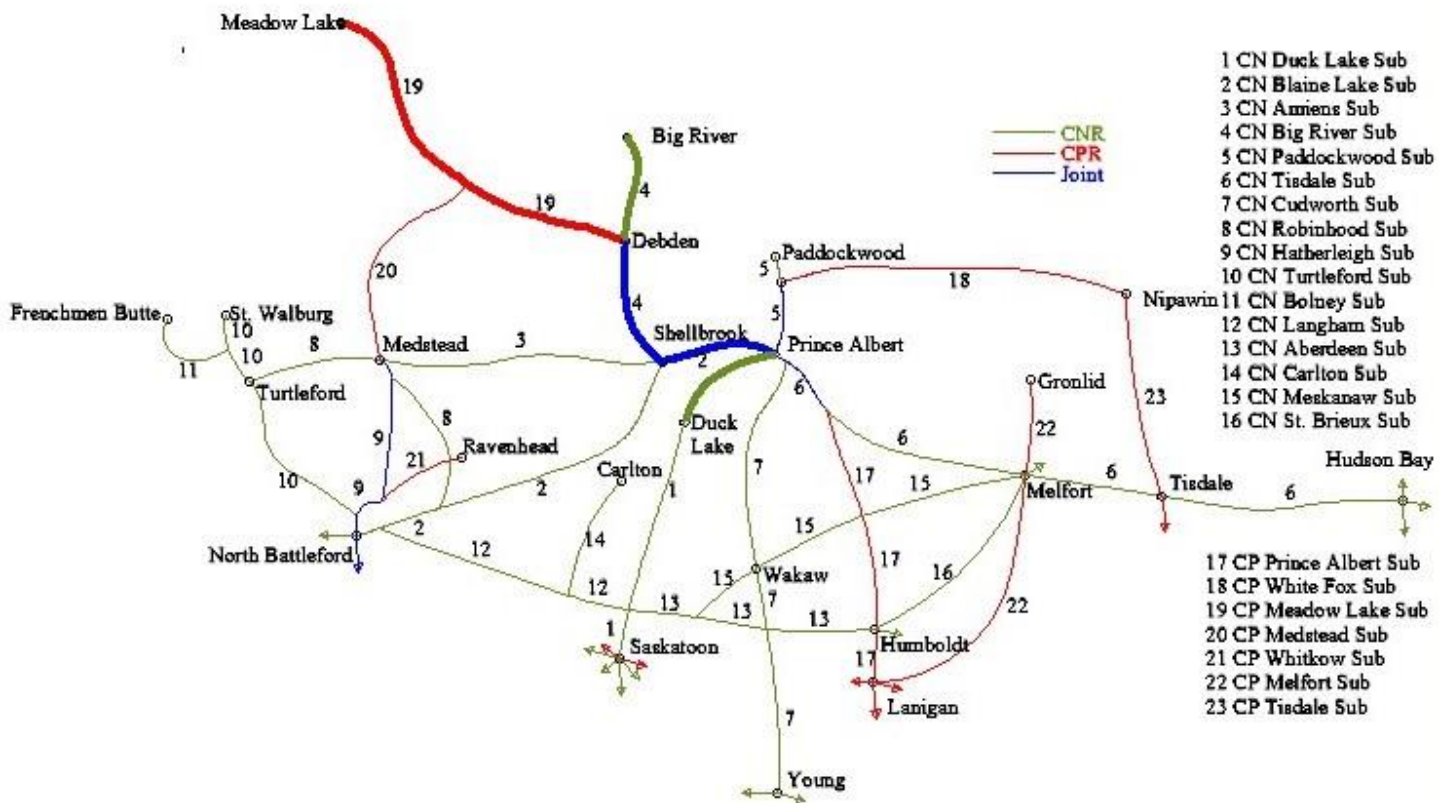


Fig. 1. Map of the prototype railroads in northern Saskatchewan and the lines being modeled.

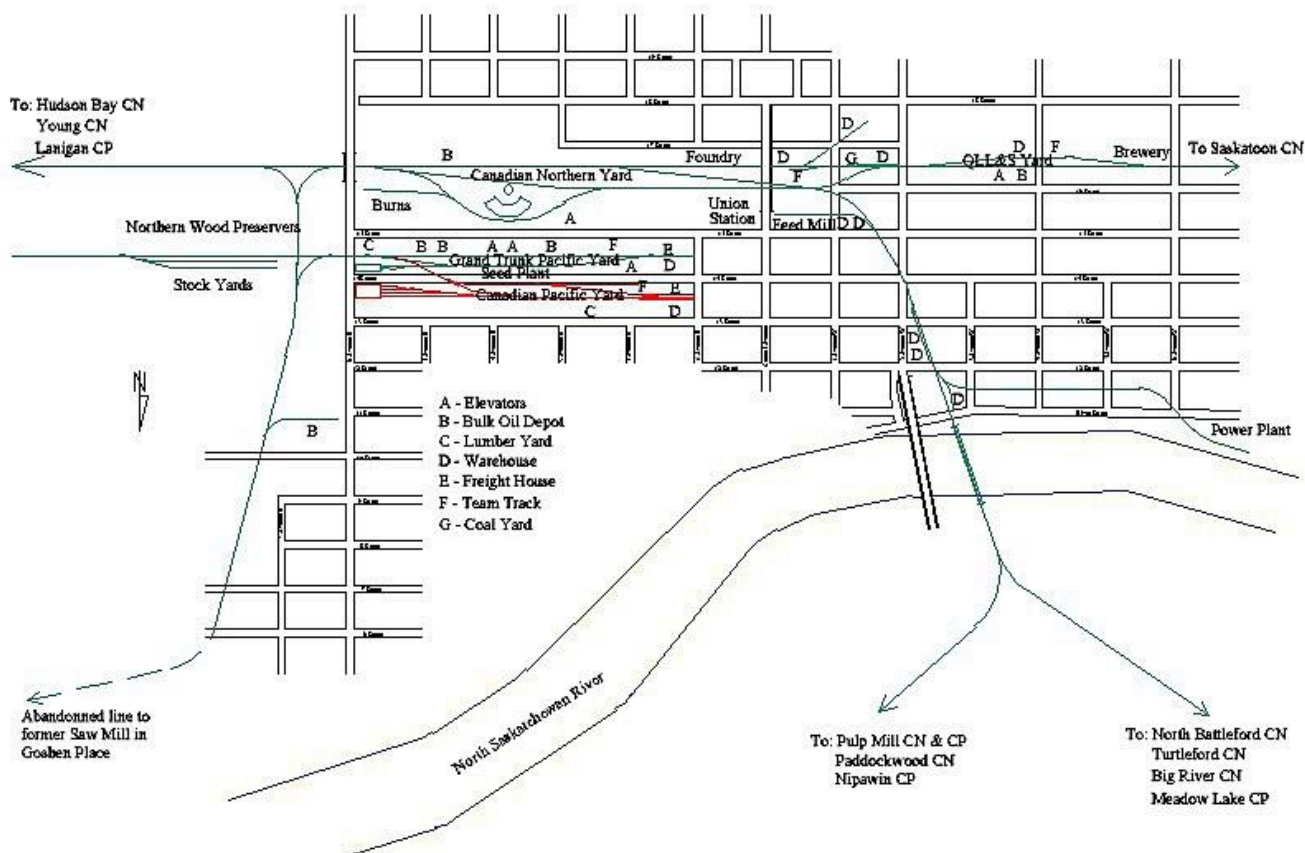


Fig. 2. CN and CP trackage in Prince Albert.

In the 1960s, the CNR ran 1000-series GMD-1s along with the occasional SW1200RS on most of the branch lines. The Duck Lake Subdivision speed freights used GP7s, GP9s, F3s, and F7s and the conventional passenger trains either 1900-series GMD-1s, which were equipped with steam generators, or freight units in combination with steam generator cars. The GPs and Fs were also used on the Tisdale Subdivision.

The CPR mostly ran RS-23s on the branch lines, although an SW1200RS might show up at times. For its speed freights, the CPR used combinations of GP7s, GP9s, RS-3s, RS-10s, and RS-18s with GP9s dominant.

The Concept

The concept for my layout is to represent the railways of northern Saskatchewan. I developed a list of priorities as follows:

1. Representation of rail lines in the City of Prince Albert including the PAPCO pulp mill complex.
2. Representation of CNR's Blaine Lake Subdivision from Prince Albert to Shellbrook.
3. Representation of some portion of CPR's Meadow Lake Subdivision.
4. Representation of some portion of CNR's Duck Lake Subdivision.
5. Representation of some portion of CNR's Big River Subdivision.

The intent is to model the mid- to late 1960s. Generally, the cut-off is just before the CP Rail "Packman" paint scheme, Great Northern Big Sky Blue, and the Penn Central. Following are some of the features I'd like to incorporate into each of my priority areas:

1. The **City of Prince Albert**: There were four separate yards in Prince Albert in the 1960s. The Qu'Appelle Long Lake & Saskatchewan yard in the west end was the remains of the original railyard. By this time, it was mainly a team track yard. The Grand Trunk Pacific yard was more of an industrial switching area by this time, although it was also used for car storage and, prior to my time period, the Canadian National freight house was located here. The main Canadian National yard was a significantly modified Canadian Northern Yard and included a classification yard, coach yard, and roundhouse. Union Station was adjacent to this yard. Fun fact: the roundhouse was the last one built in Canada in 1954. Initially, six stalls were for steam locomotives and six for diesels. The CPR yard ran parallel to the GTP yard and was anchored on one end by a freight house and office and at the other

end by a four-stall engine house. The PAPCO pulp mill east of the City was opened in 1968 and was a major traffic source, so it will be included, although maybe in staging.

2. The **Blaine Lake Subdivision** ran 25 miles west to Shellbrook, which was a three-way junction. The Blaine Lake Subdivision then continued southwest to North Battleford. The Amiens Subdivision went straight west to Medstead and the Big River Subdivision north to its namesake town. CPR trains had trackage rights over this portion of the line. As a result, these 25 miles of branch line had several trains a day. Over half of the first 25 miles went through the Nesbit Provincial Forest.
3. **Meadow Lake** is the most northerly point on the CPR and the most northerly railway in Saskatchewan, with the exception of some mining spurs that sneak over the border from Manitoba near Flin Flon. The first 60 miles of this line are trackage rights over CNR's Blaine Lake and Big River Subdivisions. Most of the line after leaving CNR trackage is in the Northern Provincial Forest; however, the City of Meadow Lake is not in the forest but in farm land. Most of the traffic sources on this subdivision are in Meadow Lake and are split between agriculture and forestry. A mixed train operated on this line until 1972.
4. The **Duck Lake Subdivision** was the original rail line to Prince Albert and served as CNR's connection between Prince Albert and the main line in Saskatoon. The trains on this line were speed freights, conventional passenger trains, and RDC's as opposed to the way freights and mixed trains of the other lines.
5. **Big River** was CNR's most northerly point in Saskatchewan. Unlike Meadow Lake where agriculture and forestry were equal, Big River was located in the Northern Provincial Forest and traffic was heavily weighted towards forestry. In some years, more grain, for animal feed, was shipped to Big River than shipped out. However, south of Big River the line served a number of agricultural towns.
6. **Other:** CNR's Tisdale Subdivision and CPR's Prince Albert Subdivision are represented by staging. CNR's Paddockwood and Cudworth Subdivisions and CPR's White Fox Subdivision are not represented.

The Layout

The layout is in a 24 x 24-foot purpose-built room that resembles a two-car attached garage. The walls and ceiling are drywalled and the floor is concrete sealed with epoxy floor paint. It is heated with in-floor hot water heating. The room was completed in late 2017 and the layout started in mid-2018. Unlike my two previous attempts at modelling this area, I decided to start with Meadow Lake rather than Prince Albert.

At this time, I have completed my version of the Meadow Lake Subdivision and about half of the Big River Subdivision. This trackage is fed by the West Staging Yard, which will ultimately represent North Battleford, but at this time represents the entire outside world and is a bit of a working yard. I have also completed about two-thirds of the Duck Lake Subdivision on the upper level. No scenery has been completed to-date but several structure mock-ups are in place. Figure 3 shows the track plan to-date.

Minimum radius is 30 inches on the main track with some secondary track down to 24 inches. In the future, some industrial tracks may curve as sharp as 18 inches. The main track on the lower deck is hand laid code 83 and sidings and spurs are code 70. The long stretch of partially hidden track between Meadow Lake and Debden is code 83 flex track. With the exception of a PECO code 83 wye turnout on the tail track of the wye, all turnouts on the lower deck are #5s built with Fast Tracks jigs and operated with Blue Point manual turnout controllers. The upper deck track is code 83 flex

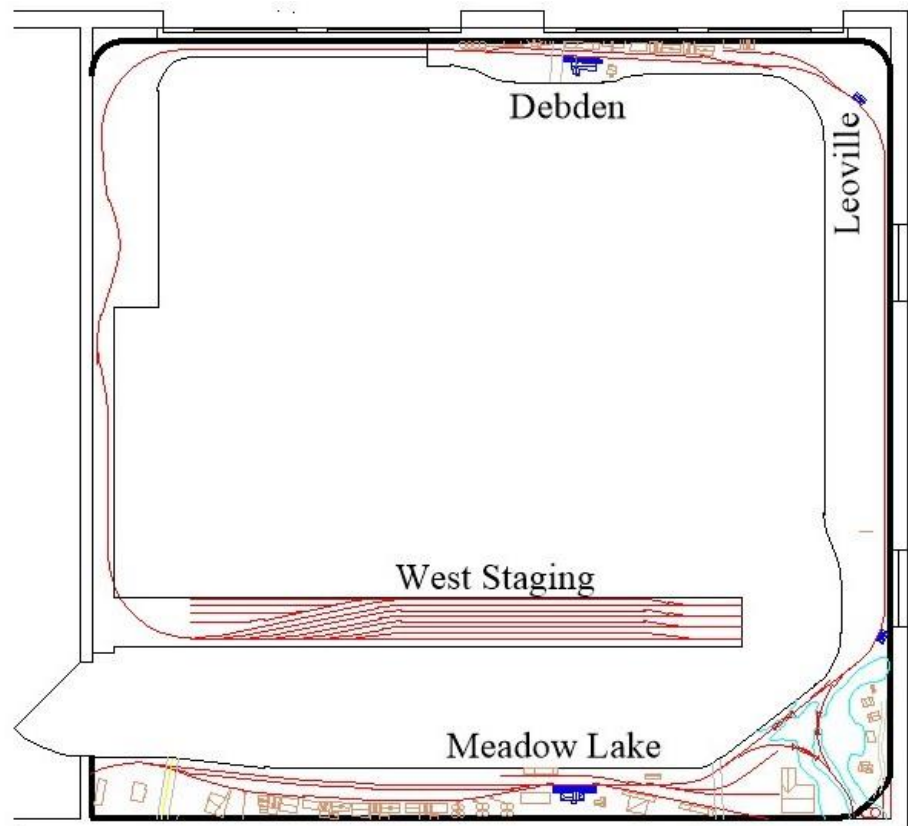


Fig. 3. Existing track plan built to-date.

Layout at a Glance

Railroad name: Parkland Division

Scale: HO (1:87.1)

Size: 24 x 24'

Prototype/theme: Canadian National and Canadian Pacific

Locale: Saskatchewan Parklands (aka Northern Grain Belt)

Era: late 1960s

Style: walk-in

Mainline run: Currently 100 feet; future 300 feet

Minimum radius: 30" main line, 24" secondary track, 18" industrial spurs

Minimum turnout: #5

Maximum grade: 2%

Train length: 6 to 12 cars on most lines; future Duck Lake Subdivision up to 20 cars

Benchwork: Lower deck open grid supported by L-girders; upper deck uses shelf brackets

Height: Lower deck 42-46", upper deck 54-62"

Aisle width: 42" average, 24" minimum, 60" maximum

Roadbed: Cork on plywood

Track: Lower deck hand laid code 70 and code 83 with Fast Tracks turnouts. Upper deck mostly flex track code 70, 75, 83 and 100 with Peco and Atlas turnouts. Some hand laid code 70 and 83 with Fast Tracks turnouts. Staging and helix code 100 flex and Peco turnouts.

Scenery: None to date; some structures and structure mock-ups in place

Backdrop: None so far

Control: DCC; currently Atlas Commander (Lenz); planning on upgrading in next couple of years

Operations: Long term: TT&TO with virtual weigh bill driven switch lists. Current: yard limits with spreadsheet-driven switch lists.

Online presence: Occasional emailed updates to select group. I need a blog!

track on the main and code 70 flex track on the sidings and spurs. Turnouts are #5 and #6 PECO code 83. The West Staging yard is code 100 flex with medium radius PECO turnouts.

Lower deck benchwork for the most part is open grid supported by L-girders. With the exception of West Staging, it is cantilevered from the walls. The upper deck benchwork is 3/4-inch plywood supported by double-slot shelf brackets.

Future Plans

Figures 4 and 5 show the proposed future lower and upper decks, respectively, and Figure 6 shows the South Staging tracks.

The next step will be to build the town of Big River. As you can see from Figure 1, the joint trackage comes to an end in Debden and the line splits with the CPR heading northwest to Meadow Lake and the CNR straight north to Big River. On the layout, I will have a non-prototypical connection from Big River to Meadow Lake which will allow CNR engines to be turned on the Meadow Lake wye. Big River is a logging town with a large sawmill. It is also home to Waite's Fisheries where fish from northern Saskatchewan are processed and sent across the continent in express reefers.

Once Big River is complete, the lower deck peninsula will be constructed and track extended to Shellbrook and Holbein. Just west of Shellbrook is the Big River Junction wye where trains will head north up the Big River Subdivision, west on the Amiens Subdivision, or southwest on the Blaine Lake Subdivision. Both the Amiens and Blaine Lake subdivisions will be represented by staging.

Shellbrook is an important operating point where trains from the Big River Subdivision will set off westbound cars. Shellbrook will have three or four elevators, one or two fuel dealers, and a fertilizer warehouse. Holbein is not much more than a wide spot on the road but did have three elevators and stock pens. After leaving Holbein, the track will enter the helix

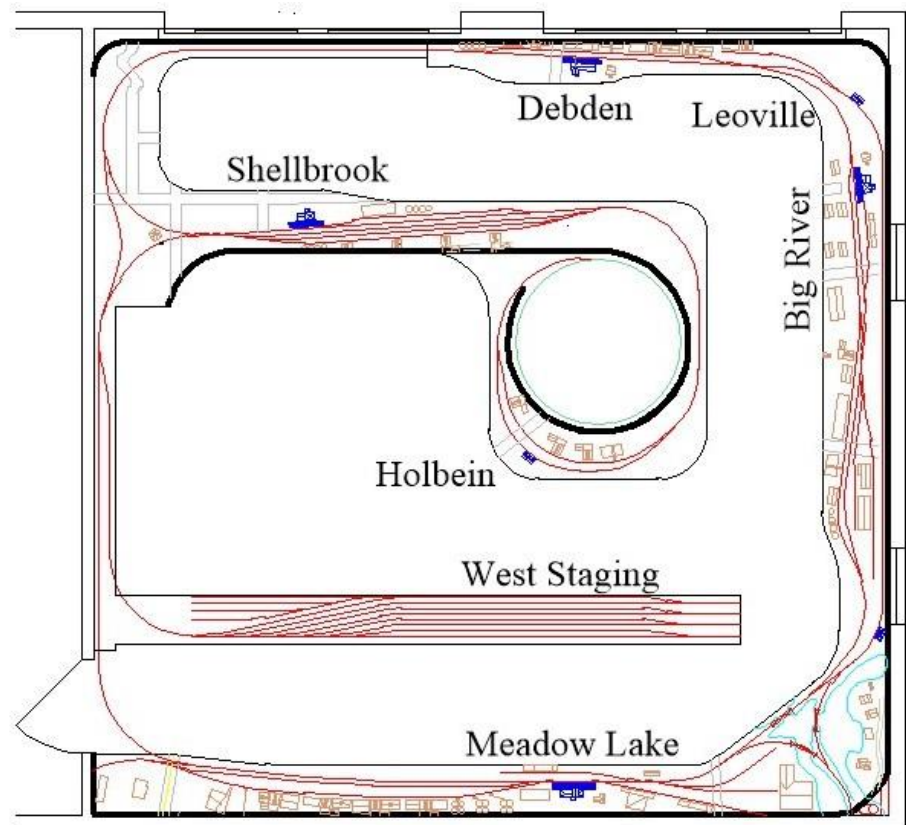


Fig. 4. Lower level track plan.

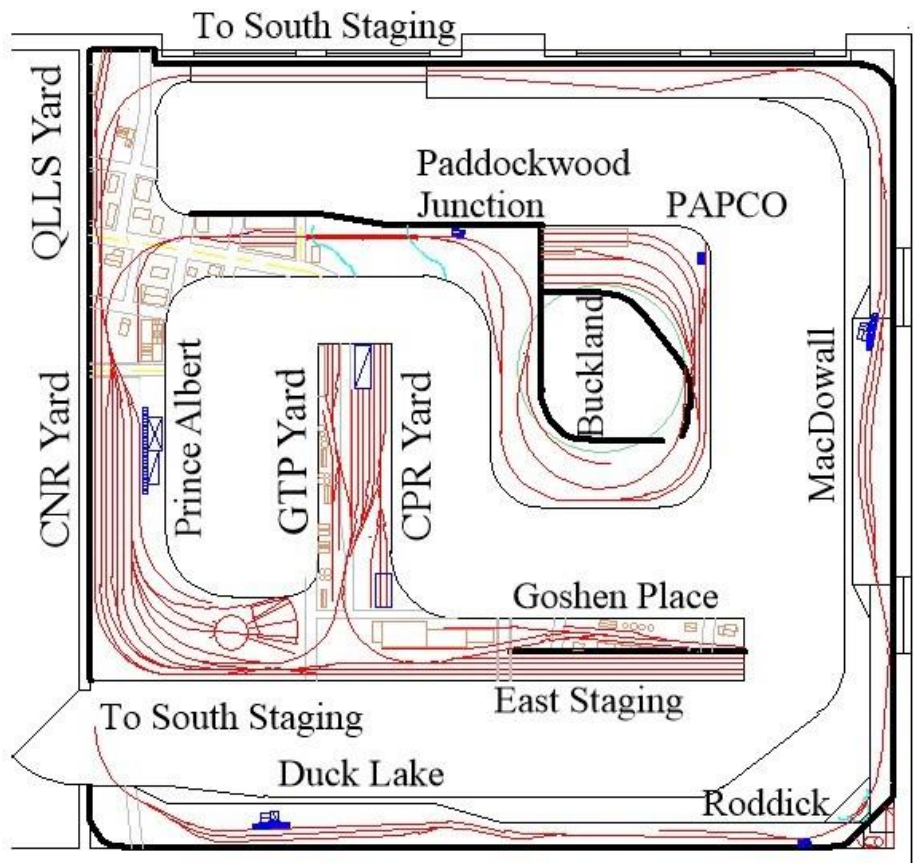


Fig. 5. Upper level track plan.

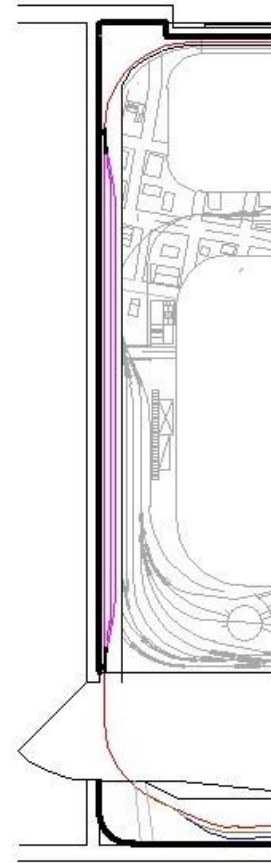


Fig. 6. South Staging.

to the upper deck.

The helix will be herniated and a short siding will be built at Crutwell. Buckland is at the top of the helix and at one time had a large ballast pit. Paddockwood Junction is next on the outskirts of Prince Albert. Both CNR and CPR yards will be modelled along with several local industries. A line from Paddockwood Junction will serve the PAPCO pulp mill.

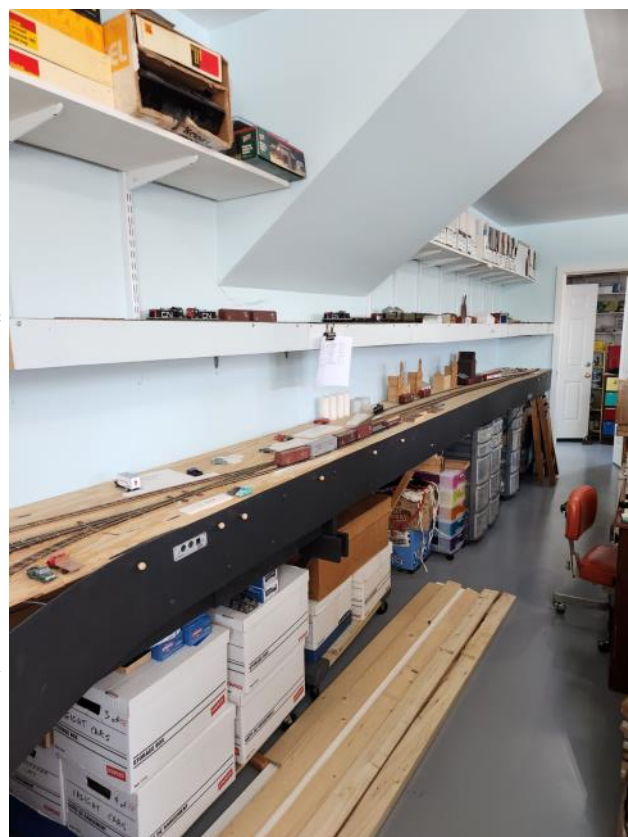
Staging representing both the CNR Tisdale Subdivision and the CPR Prince Albert Subdivision will be adjacent to Prince Albert. A long helper grade will join Prince Albert to MacDowall on the Duck Lake Subdivision. After travelling through the town of Duck Lake, the line will enter staging representing Saskatoon on the CNR mainline. MacDowall has little industry, just an elevator and a team track. Duck Lake has more, with stock pens, a bulk fuel dealer, and an alfalfa dehydrating plant.

Plans are also being made for two continuous-run connections, one for the lower deck connecting Debden, Big River, and Meadow Lake and the other on the upper deck.

Operations

The reason for building the layout is to have operating sessions. I have been having solo sessions simulating CPR trains serving Meadow Lake. Recently, I had a couple of friends over to operate this job and get their impressions, which were favourable. Once Big River is completed, CNR trains and more operators can be added.

In its ultimate form, the layout will need four train crews for the CNR and two for the CPR. Because the lines for the most part did



Meadow Lake looking West.



Meadow Lake Elevator Row.



Meadow Lake looking East.



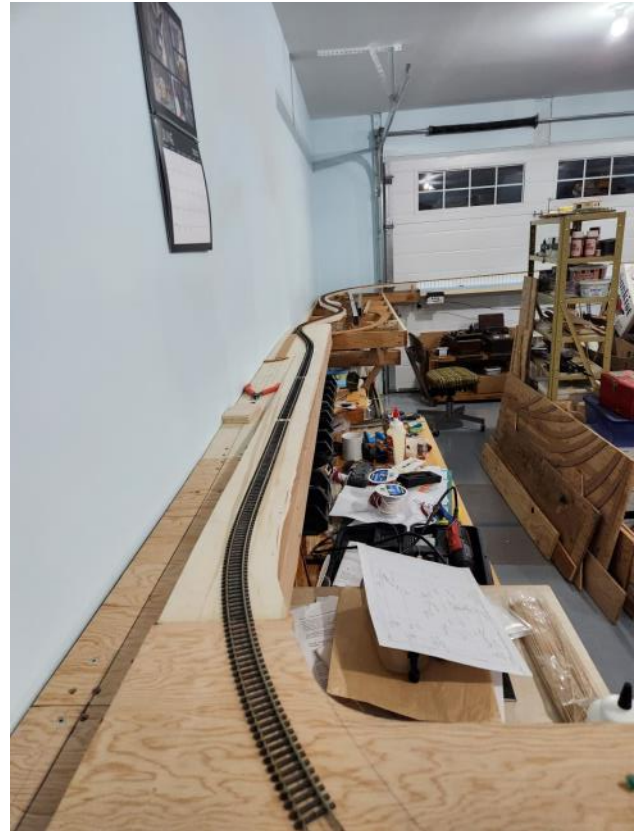
Meadow Lake freight shed and station.



Meadow Lake wye and sawmill.



Debden



Between Debden and West Staging.

not

depend on each other, I can run a CPR session with two crews, a CNR session with four crews, or a complete session with six crews.

CNR trains will include speed freights on the Duck Lake Subdivision; way freights and/or mixed trains on the Duck Lake, Tisdale, Blaine Lake and Big River Subdivisions; local industrial switching in Prince Albert including PAPCO; and passenger trains on the Duck Lake Subdivision. I may also add passenger trains to some of the other CNR lines.

CPR trains will include speed freights on the Prince Albert Subdivision; way freights and mixed trains on the Meadow Lake Subdivision; and local industrial switching in Prince Albert, including PAPCO. I may also add passenger trains on the CPR.

I hope you have enjoyed this overview of the Parkland Subdivision.

Editor's Note: Cal Sexsmith is Secretary of the [Layout Design Special Interest Group](#) (LDSIG). The LDSIG is one of many independent, non-profit organizations within the [NMRA Special Interest Group Program](#).



West Staging.

Scratchbuilding an AP Quality Structure—Part 2 of 3

Paper Interiors for a Two-Story Building

Article and Photos by Bob Parrish, MMR, 3rd Division

Editor's Note: Part 1 was published in the November 2023 Switchlist.

In the first part of this series, I showed the steps necessary to do those things that the kit manufacturers do for you. In that story, I gave tips on how to make accurate door and window cut-outs that would easily receive door and window castings from several manufacturers.

In this second part, I will offer ideas and tips for interiors that are easily accomplished.

Window Glass

It is important to select a glass material that does not draw any attention to itself. Most plastic sheet has a cloudiness to it that cannot be hidden. There is a product that can be applied with a pin or needle where it is applied around the frame of the window and gradually added until the center is filled. I have no experience with it other than seeing models that have used this material. The window surface gets something of a non-flat fish eye appearance that prevents clearly viewing an interior.

I will make a case here for real glass. Nothing looks like glass except glass. It is flat and creates a reflection that offers absolutely what the prototype would do. It is easily recognized for what it is. Plastic sheet does not, in my experience, stay flat over time. Glass once glued in does not move and will stay clear in appearance. I have successfully used diluted Windex and a Q-tip to wash windows on structures which have been on my railroad for many years.

What is most amazing about glass is that the thickness cannot be discerned when an interior is viewed through it. I will admit that the thickness of glass is a problem if a roof is removable and the glass and the interior may be seen from that vantage point. There are solutions for this that I will get to shortly. For the moment, thickness of the glass is not an issue with interiors which are only viewable from the outside.

Cutting glass is an enigma for many; but it can be mastered. The tools you will need for accurately and consistently cutting glass are pictured in Figure 2. It shows two scribes generally available at your local big box store. Made by General Tool Company, perhaps among other brands, these are not expensive and will last for years with no maintenance. They are a pencil-like stylus with a carbide tip. Glass cutters with a roller wheel are not suitable. If you shop for a scribe, try to find one with a tip ground to a 60° angle. A tip with a 45° angle will work but it is a bit harder to use because the point is obscured by the diameter of the tip. Also shown is the square that is necessary for an accurate pull with the stylus.

When making the pull with the cutting stylus, only make the pass once. Place your thumb under the scribe mark and pull down from one edge with your index fingers. If the glass resists breaking along the line, you have not made a deep enough trace over the glass. You get a pass here and can make a second pull. You will learn quickly how hard to press the stylus and what it feels like when you get a good pull. The



Fig. 1. This structure is what we built in Part 1. The interior is visible through the front windows and is the subject of Part 2.



Fig. 2. Tools for cutting glass.

problem with multiple pulls is that it shatters the surface of the glass and, when you attempt to make the break, the glass has no certain path to follow and you will get a break that runs off to one side or the other. Some of these can be cleaned up with miniature channel lock pliers. But not all.

Again, you will learn what a good pull feels like. What is also important here is that you start absolutely on one edge of the glass and run off the other while making the pull.

Microscope slides come in essentially two thicknesses, full slide and slide cover. Standard slides are about 0.015” thick while the covers are about 0.008” thick. Slide covers are useful if you have a removable roof and can look in as they are so thin that they can be glued in and the glue hidden by using common bond paper as a wall paper to cover the interior. In cutting slide covers, the stylus hardly needs to be pressed down. The glass is so thin that the weight of the tool is often enough to make the scratch. Frequently it will break clean along your scribe mark and hand you the completed glass piece.

Another aside here for a moment pertains to securing the glass in your structure. Not many adhesives will stick to glass. Some are designed to get into the fiber of the material to be bonded. Glass has no fiber so there is nothing for the adhesive to get a grip into. Some adhesives are just plain sticky and will hook up to most anything. This is what we need for securing glass into your structure. Additionally, even styrene cement will not hold the glass in place unless you catch the corner and bond it down with something like a corner tab in an old photo album.

The solution is found in an adhesive that has two identities that are almost alike. Contact cement and Walther’s Goo®. There is another very similar product and it is from a company called Zap® and their product is also called Goo.

When using any of these products, if you get a smear or a hair-like filament of it on the glass, wait for the glue to totally cure and it will clean up with an X-Acto blade.

After all of that, let’s get to the interior.

Interior Structure

The wall material used here is called mat board. This is available in many colors in art supply stores. Here I used white and it cuts easily with a common X-Acto blade. The product is about 1/16” thick and takes glue easily and will, for these purposes, take glue well on the edge as seen in the following photos.

Figure 3 is the ground floor interior wall structure that has the two stores and the center stairwell. The notches in the lower floor provide clearance around the inset door entries. This can be roughly cut as there is no way to see this area through the store front windows. The space behind the display rooms serves two purposes—it avoids the corner reinforcing of the larger building and it provides a place to gather up the wires for the lighting that is throughout the building. Additionally, these interior walls prevent light behind the structure from showing through to the front. Few prototype buildings have a continuous view through an entire structure.

Finishing the Interior

Next, the two floors are roughed in without a lot of detail as yet (Figure 4). The flooring of the first floor is nothing more than the results of a Google search of hardwood floors downloaded as a JPEG file and then dropped into a Word file, sized, and printed out. I use common white glue for putting down the paper flooring pieces. The second floor is done in a similar method with only a slightly different color. The striped wall paper is the product of a Google search for a stripes pattern and then printed out, as was done for the flooring. At this point, you are starting to get a notion of how I am getting this done. The chair rail molding around the second floor rooms is scale 1x4” stripwood pre-colored before installation.

I used some premade stair steps and railing from Central Valley Model Works for the central stairwell to the second floor. Although this goes largely unseen, it was an important step as the second floor of this is pretty visible through the second story bay window. After that, I know it’s there.

Also seen in Figure 4 are the close edge walls that are not very cleanly cut. None of this is visible through

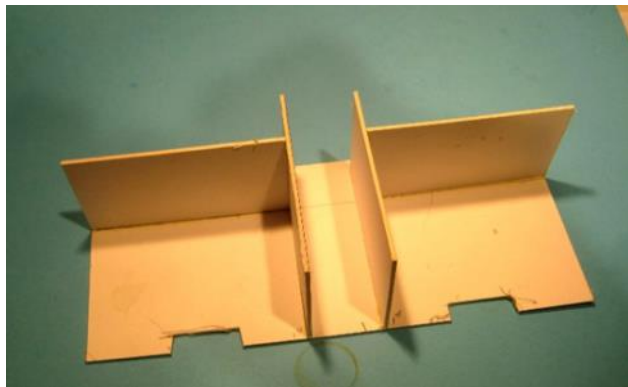


Fig. 3. Interior walls for the ground floor.



Fig. 4. Interior finishing has started with floor and wall coverings.

the windows, so I got over the raggedy appearance. I have not used heavier weight card stock on any of this.

Paper interiors are just that, paper. You will discover that, once the viewer looks through a layer of glass, depth perception is almost totally lost. We can use that phenomenon to our advantage. For this structure, there is a dry goods store and a ladies millinery store. For the moment, let's go after the commonalities of the two stores.

Figure 5 shows a couple of screen shots based on a search query called "old dry goods store." And then I downloaded one particular photo of interest. Although I have a photo editor, Microsoft Word (and the later version 360) allows you to play with some photos and get a good enough print to be able to do most anything.

From here in a text file, the photo can be pulled up or down in size and then printed to the scale and size desired.

The smaller pull-outs of a particular photo are what will be printed and used on the back walls of the ground floor.

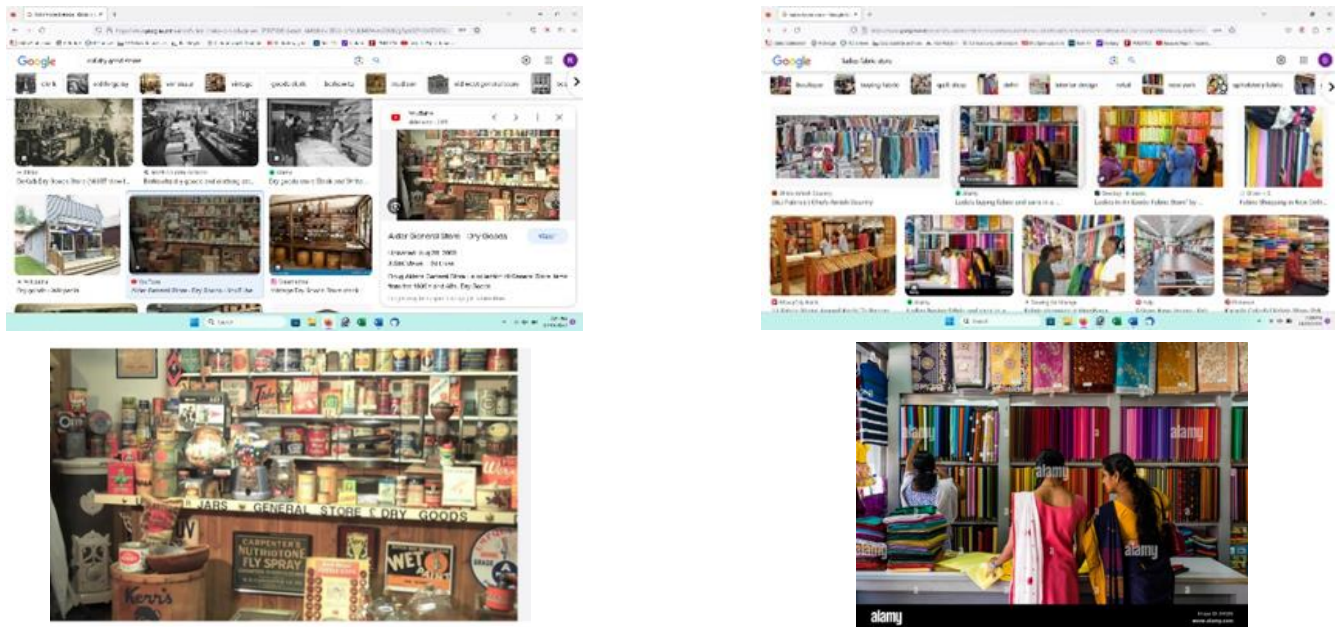


Fig. 5. Results of Google searches for images of store interiors.

Adding Details and Lighting

The two photos below (Figure 6) show some second floor furnishings and people that offer detail, interest, and a "lived in" appearance that makes the scene believable.



Fig. 6. More details being added. Now it's really getting interesting!

The wiring for the first and second floors are all Miniaturics 16V bulbs running on 12.5 volts. This creates a yellowish glow that cannot otherwise be accomplished. The period of time I model did not have truly white interior lighting. Figure 7 shows how I hid the wires and junctions in the voids that cannot be seen from the street-level windows. The outer pairs of windows in the rear wall are lit and the two closest to the stairwell are dark and there is no need to explain to the viewer why that might be. Further, where this building is located on the layout, the rear wall is completely out of sight anyway. The only wiring coming out of the bottom of the structure is a single pair of wires that drop through the benchwork and connect to the nearby lighting bus wire.

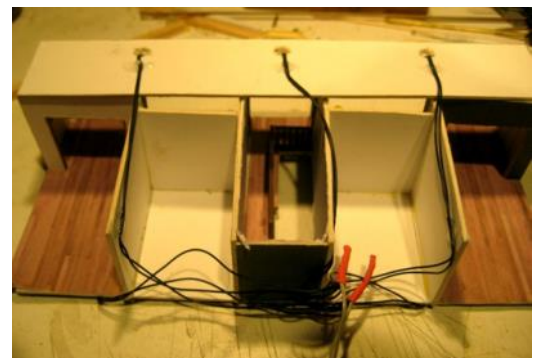


Fig. 7. Interior lighting.

Figure 8 shows what the viewer will see through the store front windows. In addition to the printed paper back wall details, I made up some really simple counters from 0.025" styrene sheet and colored them brown to look like wood (Figure 9). As these would only be seen from one direction through a window, I paid little attention to the back sides of these.



Fig. 8. The interior is coming together with more details added.

I put a bit of clutter on the counters that really only took up space and created some look of activity. A Coke bottle crate is visible in the right side window here. The wall phone also adds interest when viewed up close. There is a figure of a lady standing in front of a counter. All of this adds depth without really defining what might be going on.

In the millinery store (Figure 10), a mannequin is positioned in the right side window. This is nothing more than a figure with the arms and legs cut off. It offers depth. In the left window is a lady that could be considered a customer or another mannequin. There is no need to define it; the addition of depth is all that is necessary. A figure is standing at the counter and another behind it with fabric laid out in front of them. It is nothing more than colored paper. The observer from the edge of my railroad cannot make out such detail but the overall scene makes it all believable.

Figure 11 provides a look into the second floor. A little clutter and some people make the whole thing interesting.

What makes these interiors work is there is a bit of a three-dimensional appearance with items in the windows and a few people standing between the glass windows and the back walls. This offers some depth without divulging how really shallow the entire interior really is.

In reality, there is so little truly visible that the “mind’s eye” rather takes over and invents what might actually be going on here.

Years ago, a good friend told me that all of modeling was an illusion. I think this is what interiors are all about. Let the viewer decide for themselves what they think they see.

This then is my take on simple interiors. I chose not to get caught up in shades or curtains as there was enough going on that, until the absence is pointed out, they go essentially unneeded. I hope you found some ideas here that may inspire you to go back and open the curtains and show us what is inside of your buildings.

In part 3 of this story, I will explore scratchbuilding with brick materials and some of the ways to get some great looking masonry buildings.

Editor’s note: Watch for Part 3 in the May 2024 Switchlist.

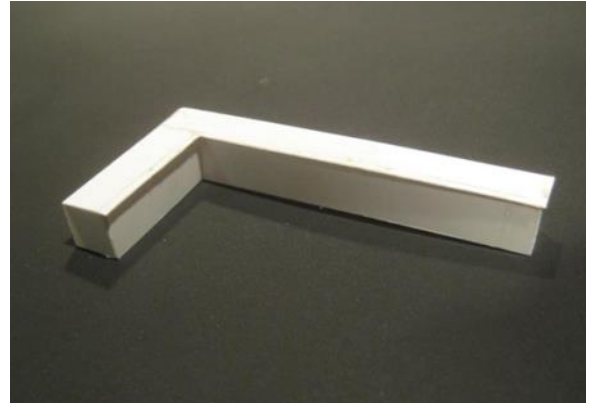


Fig. 9. A simple scratchbuilt store counter.

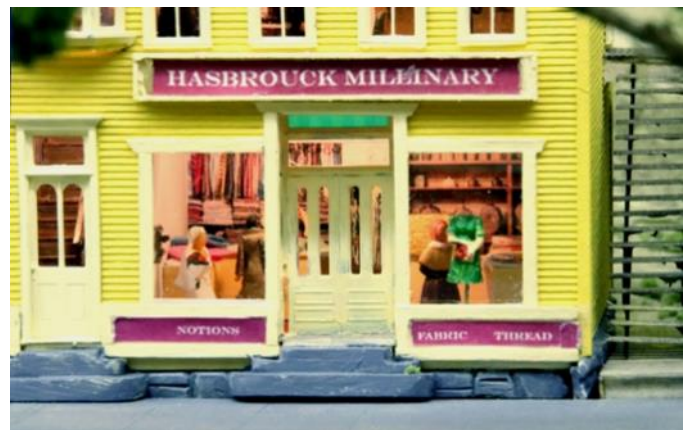


Fig. 10. Interior of the millinery store.



Fig. 11. A peek into the second floor.

Installing Turnout Controls on a Free-Mo Module

Article and Photos by Brian Orysen, 1st Division

I'm a member of the Umpqua Valley Model Railroad Club (UVMRC), which is located in Roseburg, Oregon. We have a large number of [Free-Mo](#) modules which needed turnout controls installed on the modules. If you're not familiar with Free-Mo, it was developed with the idea that a set of standards focusing mainly on module endplates would enable faithful modeling of prototype track plans and operations in a modular environment.

In our situation, there is no Free-Mo standard or recommended practice addressing the design of turnout controls, but do specify where they are to be located. The relevant standard and recommended practice are:

- S5.2 For a given turnout, turnout controls must be on all sides of the module or module section, excepting any endplates.
- RP5.2.1 Turnout controls should be located on the fascia, and not on the horizontal or vertical surfaces of your scenery.

So, we had quite a bit of freedom in the design of our turnout controls, which was really helpful because we had several objectives to achieve.

Objective 1. Ease of assembly and installation.

Solution: The assembly of the unit can be done at a workbench and multiples built at a time. Plus by using the same base design mounting system, it can be easily installed in any module.

Objective 2. Prevent damage of turnout controls when storing, transporting, and during setup/tear down of modules.

Solution: By using PVC test caps for mounting, the controls are recessed to protect toggle switches, push button switches, and LEDs.

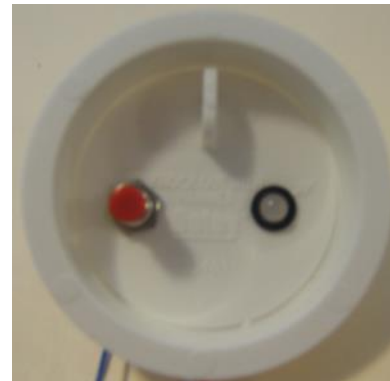
Objective 3. Ease of repair and replacement if damaged or inoperative.

Solution: Turnout control can be unwired and removed from the module easily and replaced with a new one, due to the generic build. Then the damaged one can be repaired later at the workbench.

Objective 4. Provide for different turnout control configurations, but still using the same base design footprint.

Solution: The club uses Oatey® 2-inch test caps (also available in 1½ inch). We found the Oateys are deeper and have pre-formed arrows for reference of drilling holes for centering in the cap. Below are two examples that the club uses.

To control two turnouts with a smaller installation footprint, the club has replaced the LED with another toggle switch. This design is used where a turnout indication light is not needed, especially in yard locations.



Left. Manual Tortoise Control with DPDT toggle switch with red/green 2-lead 5mm LED with holder.
Right. DCC Tortoise Control with normally-open push-button switch with red/green 2-lead 5 mm LED with holder.

Officers' Reports

From the Engineer's Side of the Cab

Jeff Herrmann, President

Now that we are at about the “halfway” point of the winter (at least here in north Idaho I hope), I know that most of you, like me, are getting knee deep in a few modeling projects that you have been thinking about for some time. As I mentioned in the November *Switchlist*, I have prioritized some personal projects related to getting my home layout up and running, finishing a few models needed for AP evaluation, and thinking about bringing a model or two to the PNR Convention for display.

Our PNR Board of Directors has been very busy over the past few months as well. [Surrey Excursion](#), the PNR Region Convention to be held in May at the Sheraton Guildford Hotel in Surrey, British Columbia is filling up fast and the program features are looking really exciting! Please see the related articles on the convention in this *Switchlist*... It looks to be the largest and most exciting convention we have had in some time, so don't miss it!

For several months, I have been reporting to you about the creation and rollout of the *Pathfinder Program*. It looks like we are getting some traction and making good progress. I am certain that the assignment of a *mentor*, *big brother*, or *PATHFINDER* for new members is critical to establishing the bonds essential to a relationship that leads to engagement and continued affiliation with the Association. Far too often, new members are left to their own devices to figure out simple stuff including how to get involved or how to get help needed with their modeling. We recognize that sending emails and generic electronic invitations to events and activities alone is not an effective means of contact. We need to achieve a “personal connection” between every member and the Association. Please recognize that we are not trying to contract with individuals for some level of service, but rather we are trying to create a personal relationship that has, at its heart, our mutual enjoyment of the great hobby of model railroading! Suffice it to say that our new members won't care about us unless they know we care about them. If you would like to help in this effort by opening your home or club layout, leading a clinic, or becoming a Pathfinder in your local area, please let your Division Superintendent know of your interest!

On a related note, this past December we started our New Member Welcoming Zoom Meetings. For about an hour each month, PNR has a scheduled Zoom Meeting to introduce new members to the NMRA and information on how to find out what is going on in their area. A personal invitation to participate and have their questions answered is made to each new member. If you would like to see what the meeting looks like, here is the link to the meeting we held in December: https://youtu.be/emRpxS1_Yps?si=JqKVwITKZiojG-yy

If you have some ideas you would like to share, or think that I can be helpful in getting NMRA members more engaged or expanding our membership in your area, let me know. My focus, and that of our PNR Board of Directors, is to find ways to enhance the value of your NMRA membership.

I wish each one of you good health and clear tracks ahead! I am humbled and excited to serve as President of Pacific Northwest Region.



From the Fireman's Side of the Cab

Richard Kürschner, Vice President

No report this quarter.

Treasurer's Report

Rick Martin, MMR

Greetings fellow Region members.

The past calendar year was a good one for this group. From a financial position, the PNR had an excess income of around \$750. To what do we owe this outcome? Well, my first answer is better accounting tracking and reporting. However, it is due to the leaders of this august organization who watch their budgets and look for ways to generate income on activities. So BRAVO ZULU to them, the leaders of this excellent organization. The Region as an organization does not generate income; rather the income comes from the Divisions primarily from the annual convention. That convention is for

all PNR members to attend.

There were a couple of both positive and not so negative surprises. On the positive side, we renewed our liability insurance with a new broker who does not charge a brokerage fee. This was a saving of just over \$200. But on the not so negative side, the PNR became responsible for our Constant Contact program, which was formerly paid for by one of our longtime members on his own nickel. The fee was just over \$600. But even with this change, we kept more bucks in the bank. Kudos to all who made this happen.

While I applaud all who made this a successful year financially, it is just one year of more to come. The job of treasurer is not a difficult one, but it does require attention to detail. Having spent my professional career in the engineering field, detail is what I did on a daily basis. Why do I say this? This is my second year as treasurer and I plan to transition the job to another eager money person to carry on. That occurs in late summer when the officers change. This position is appointed by the President, whomever that may be. My plan is to step aside at that time. I have attempted to simplify this job with simple Excel spreadsheets, a treasurer duties list, and a few other things to make the job easy. If any of you are wanting to take on the cloak, please make it known to the President. I will carry on until a successor is appointed.



Having said all the above, if any of you have questions, I would be more than happy to answer. Send me an email at rick.martin5727@gmail.com.

Respectfully submitted,
Rick Martin, MMR
PNR Treasurer

Office Manager's Report

James Keena

This report to the Members of the Pacific Northwest Region, NMRA, covers the months of January 2023 through January 2024. As you can see in Table 1, the Pacific Northwest Region, which is made up of seven (7) divisions, currently has a total of 1,396 members, of which 26 are Rail Pass members. Unfortunately, we had a loss of 23 members due to death. Table 2 shows that we brought in 221 new members over this same period, of which 47 are Rail Pass members.

Remember —Membership retention is the job of every member!!

Much of this report (Tables 1 through 4) centers on the relationship when individuals come on board as a new member and the time they go on the 30-60-day Past Due Reports. Look at the tables to see how your division is performing.

Table 1. Membership showing Rail Pass (RP) portion by Division

Div	Jan-23		Feb-23		Mar-23		Apr-23		May-23		Jun-23		Jul-23		Aug-23		Sep-23		Oct-23		Nov-23		Dec-23		Jan-24		Net +/- over year Jan-Jan
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	
1	80	0	80	0	79	3	79	3	79	3	80	4	79	3	79	3	80	5	72	5	82	5	84	3	85	1	5
2	120	0	118	1	119	1	115	1	112	1	111	1	112	1	115	1	110	2	110	1	110	1	110	2	108	2	-12
3	65	0	61	2	65	3	74	3	65	3	67	3	66	3	66	2	66	5	64	5	67	6	69	5	69	5	4
4	445	0	429	10	436	12	432	15	431	17	429	16	435	15	430	15	423	15	440	12	434	12	433	12	414	9	-31
5	193	0	185	4	180	3	175	4	179	3	182	4	186	4	189	3	177	4	186	5	188	5	181	4	196	3	3
6	201	1	204	2	198	2	198	4	204	5	211	5	219	4	259	3	258	4	262	2	260	2	258	1	258	1	57
7	259	0	257	1	263	2	261	3	254	2	255	3	255	3	256	3	264	4	267	4	265	3	263	2	266	5	7
Tot	1363	1	1334	20	1340	26	1334	33	1324	34	1335	36	1352	33	1394	30	1378	39	1401	34	1406	34	1398	29	1396	26	33

Table 2. New members showing Rail Pass portion by Division

Div	Jan-23		Feb-23		Mar-23		Apr-23		May-23		Jun-23		Jul-23		Aug-23		Sep-23		Oct-23		Nov-23		Dec-23		Jan-24		Total	
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP
1	0	0	1	0	1	1	3	0	0	0	0	1	0	0	1	1	1	1	2	0	1	0	2	0	0	0	12	4
2	2	0	0	0	1	0	5	0	0	0	0	0	0	0	2	1	1	0	1	0	0	0	0	1	1	0	13	2
3	0	0	0	0	1	1	5	0	0	0	0	0	0	0	0	0	0	3	6	0	0	2	0	0	0	0	12	6
4	5	0	2	2	5	4	24	4	4	2	0	0	2	0	0	0	2	2	0	1	2	0	1	2	6	1	53	18
5	2	0	1	0	0	1	13	1	3	0	0	1	1	1	0	0	0	0	0	1	1	0	1	0	5	0	27	5
6	3	0	6	2	1	0	9	2	4	0	4	0	0	0	33	0	1	1	1	0	0	0	1	0	4	0	67	5
7	0	0	1	0	5	1	14	1	1	0	0	0	2	1	0	0	8	1	3	1	1	0	0	0	2	2	37	7
Tot	12	0	11	4	14	8	73	8	12	2	4	2	5	2	36	2	13	8	13	3	5	2	5	3	18	3	221	47

Table 3. Members 30-days Past Due showing Rail Pass portion by Division

Div	Jan-23		Feb-23		Mar-23		Apr-23		May-23		Jun-23		Jul-23		Aug-23		Sep-23		Oct-23		Nov-23		Dec-23		Jan-24		
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	
1	1	0	1	0	1	0	2	0	0	0	0	0	3	0	2	0	0	0	1	0	1	0	0	1	2	0	
2	2	0	1	1	2	0	4	0	2	0	1	0	0	0	0	0	5	0	2	1	0	0	2	0	1	0	
3	1	0	2	0	1	0	2	0	1	0	0	0	1	0	1	1	1	0	3	0	0	1	0	0	0	1	0
4	7	0	10	1	10	1	8	0	8	0	3	1	5	0	7	1	15	1	4	1	9	1	2	1	10	4	
5	1	0	5	0	3	1	10	0	3	1	0	0	2	0	0	0	1	0	3	0	2	0	1	0	1	1	
6	1	0	14	0	9	0	3	0	1	0	2	0	1	1	3	0	1	0	2	0	1	1	4	1	3	0	
7	2	0	2	0	2	0	7	0	10	0	0	0	2	1	3	0	0	0	1	1	4	1	4	0	2	0	
Tot	15	0	35	2	28	2	36	0	25	1	6	1	14	2	16	2	23	1	16	3	17	4	13	3	20	5	

Table 4. Members 60-days Past Due showing Rail Pass portion by Division

Div	Jan-23		Feb-23		Mar-23		Apr-23		May-23		Jun-23		Jul-23		Aug-23		Sep-23		Oct-23		Nov-23		Dec-23		Jan-24	
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP
1	0	0	1	0	1	0	1	0	2	0	0	0	0	0	2	0	1	0	0	0	1	0	1	0	0	1
2	0	0	2	0	1	1	2	0	4	0	3	0	0	0	0	0	0	0	4	0	2	1	0	0	2	0
3	0	0	1	0	1	0	1	0	1	0	0	0	0	0	1	0	1	1	1	0	3	0	0	1	0	0
4	4	1	5	1	8	0	10	1	7	0	8	0	3	1	6	0	6	1	0	0	4	1	7	1	3	2
5	2	1	1	0	4	0	4	1	4	0	4	0	0	0	1	0	0	0	1	0	2	0	2	0	1	2
6	2	1	0	0	13	0	9	0	5	0	1	0	2	0	1	1	3	0	1	0	2	0	0	1	4	1
7	11	0	2	0	2	0	1	0	6	0	7	1	1	0	0	1	3	0	0	0	1	0	3	1	4	0
Tot	19	3	12	1	30	1	28	2	29	0	23	1	6	1	11	2	14	2	7	0	15	2	13	4	14	6

Achievement Program Report

Bill Fassett, AP Manager

CONGRATULATIONS TO OUR NEWEST MMR – Brian Ferris

In December, MMR #751 was awarded to Brian Ferris of Olympia, WA. I was pleased to see Brian’s layout at the convention when he held an operating session. Brian finished the requirements for the MMR by receiving an AP Certificate for Master Builder – Motive Power, an award earned by less than 4% of all AP certificate holders. Brian’s other achievements include Master Builder – Structures, Master Builder-Scenery, Model Railroad Engineer – Civil, Model Railroad Engineer – Electrical, Chief Dispatcher, and Association Volunteer.

2024 CONVENTION

Plans for the 2024 Convention in Surrey include a change in how AP evaluations are scheduled. Attendees will be given a choice of 30-minute appointment windows on Thursday or Friday, and will be present when their model(s) are evaluated. Evaluations will be open to all attendees to observe.

Recognition of MMRs awarded since Tacoma will be made at the banquet, as was done in Tacoma.

PNR LEADS THE NMRA IN AWARDS ISSUED IN 2023

According to Frank Koch, AP Committee General Manager, a total of 323 AP Certificates were issued worldwide in 2023. Of those, 63 were issued to modelers in the Pacific Northwest Region, or about 1 in 5. The next highest number of certificates issued (36) were in the Mid-Central Region, where Frank Koch is the Region AP Manager. The 6 MMRs earned in the PNR also led all other regions (next highest was 3), and accounted for 1 out of 4 MMRs earned worldwide in 2023.

Association Volunteer: 25% of PNR awards were generated by two local club leaders! Our own records show that the region awarded 69 certificates. Of those, 20 were for Association Volunteer. Of the 20 recipients, 11 came from the Bremerton Northern Model Railroad Club and 6 were from the Tri-Cities Model Railroaders. Major thanks go to Jack Hamilton in Bremerton and Phil Miller in Tri-Cities for leading their fellow club members to achieve these awards!!

Golden Spike Award: PNR was in third place for Golden Spikes, with 8; the lead region was the Sunshine Region with 23. I remind you all that any NMRA member may validate a Golden Spike Award application.

Awards Earned since the last *Switchlist*

Merit Awards—The following members have earned Merit Awards:

Ross Dando—Cars (3), Structures
William Fassett—Cars (5)
Brian Ferris—Motive Power
Dan Figy—Cars (4)
Clyde Queen, MMR—Motive Power
Jerry R. Thomas—Structures

Achievement Program Certificates—The following member have earned AP Certificates:

Brian Arlt—Association Volunteer
William Braidic—Association Volunteer
Ross Dando—Model Railroad Author
William Fassett—Master Builder – Cars
Brian Ferris—Master Builder – Motive Power; Master Model Railroader® #751
George Fuchs—Association Volunteer
Doug Hicks—Chief Dispatcher
Greg Kujawa—Model Railroad Engineer – Electrical

The AP Team congratulates these superior model railroaders!

Featured Category: AP Certificate Master Builder – Motive Power

Modelers earning the status of Master Model Railroader (MMR®) will have earned AP certificates in each of four major areas: Model Railroad Equipment, Settings, Engineering and Operation, and Service to the Hobby. The Motive Power category belongs to the first area, Model Railroad Equipment, along with Cars. In 2023, 9% of all AP certificates worldwide were for Cars and 3.4% were for Motive Power.

Evaluated according to a matrix

The Motive Power category is one of three categories where the model is evaluated according to the NMRA Achievement Program Assessment Guidelines: Motive Power, Cars, and Structures (<https://www.nmra.org/sites/default/files/education/achievement/pdf/2006-judging-guide-lines.pdf>). Evaluations of models in these categories is based on matrices (also known in educational circles as “rubrics”) covering five aspects of model building:

- Construction – How complex or difficult was it to build the model, and how well the modeler completed the model? [0-40 points]
- Detail – How many detail parts were added to the model, and how complex or difficult was it to add the detail? [0-20 points]
- Conformity – How well does the model reflect the real world? If the model is based on a prototype, does the model have the design features of the prototype? If the model is freelanced or kitbashed, how logical is the result? [0-25 points]
- Finish and Lettering – How accurate and complete is the lettering and painting of the model, and how well is the finish (painting, texture, weathering) applied? How much lettering is present? [0-25 points]
- Scratchbuilding – How much of the model is built from scratch, and how difficult was the scratchbuilding? A model in which more than 90% of the parts are scratchbuilt is considered a “scratchbuilt model” for the purposes of the AP certificate. [0-15 points]

To qualify for a Merit Award, a model must score at least 87.5 points out of a total of 125. This is 75%, or the equivalent of a “C” grade when most of us went to school. **A “perfect” model is not required.**

Super detailed

Models that do not qualify for a Merit Award are still eligible for completing the requirements of the certificate if they are well-constructed and “super detailed.” A model built from a kit showing no extra effort to customize is unlikely to meet this requirement. However, adding relevant details, such as brake details, prototypical weathering, interior lighting, air hoses, interiors, and diaphragms should help meet this requirement. Many of the models on our layouts are designed to look realistic from 4 to 6 feet away; a super detailed model will survive a much closer inspection.

What is “motive power?”

According to the NMRA website, *motive power* is just about anything that runs on rails and is self-propelled. Examples include: steam, diesel, or electric locomotives; street cars; MOW vehicles; or traction units; and speeders. Hand cars are not self-propelled, and dummy locomotives do not qualify. Models must be capable of self-propulsion on track of the same gauge as the model.

Requirements for an AP Certificate

- A total of three models –
 - One must be scratchbuilt, meaning more than 90% of parts are made by hand. Scratchbuilt portions of the models must include:
 - For steam locomotives: frame, boiler, cab, tender frame and body, and either valve gear or main and side driving rods
 - For other motive power: body, frame, cab, power truck side frame, and pantograph or trolley poles, if applicable.
 - Excluded parts from the definition of scratchbuilt include: motor, gears, drivers and wheels, couplers, lights and electronics, trucks, paint, decals, bells, marker and classification lights, brake fittings, and any basic shapes of wood, metal, plastic.
 - Non-scratchbuilt cars must be super detailed using commercial or scratchbuilt parts.
- All of the models must receive Merit Awards

Some observations on Motive Power

- Many of the recent scratchbuilt models I’ve seen have been models of locomotives or other equipment that has a largely rectangular body structure, which may be more straightforward to construct. Consider that a speeder can be a motive power model.
- Scratchbuilding can include creating the frame or other parts by 3D printing PROVIDED that the modeler created the design, AND that the quality of the print is excellent.
- Self-propelled does not mean electrically powered. Obviously, live steam will count, but even a wind-up mechanism would qualify.
- Merit Award-winning models that are not scratchbuilt have frequently been lot bashed or extensively modified to create a model that is not otherwise commercially available.
- In general, merely repainting and lettering a RTR model will not be sufficient for a Merit Award.
- Merit Awards are most likely to be earned by models that are determined from the beginning to be evaluated..

Relationship to other award categories

If you have built motive power based on a prototype picture, and have pictures of those cars in a prototypical scene, you have the basis for a submission in Prototype Modeling. Adding lighting or a decoder to your motive power model can aid in completing the Electrical certificate.

Paperwork Reduction

The national AP Program Manager does want to see all the paperwork required by the SOQ. Pictures of the model are helpful but not required.

Selected Pictures

Here are two contest winners from the Tacoma Convention (see *The Switchlist*, August 2023). The No. 12 was both 1st place and People’s Choice and was submitted by Donald Rose. The No. 67 won 2nd place and was built by Byron Osborn.



And here is Brian Ferris's scratchbuilt model that completed his MMR (at right is a photo of the prototype – no, that's not Brian driving it).



Attention All PNR Members! Have you taken the PNR Survey yet?

Even though you may no longer consider yourself a “new” member of the National Model Railroad Association, we need your input to help us develop and plan programs, events, and activities that meet your needs as a member of the NMRA in your area. Please take just a couple of minutes to fill out our on-line survey at:

<https://www.surveymonkey.com/r/K98F93Q>

Thank you in advance for participating in this survey to help the PNR deliver benefits to you!



NMRA Video Library

Did you know the NMRA Video Library has over 150 videos of clinics that were presented at National Conventions from the last 30 years. Virtually every model railroading topic you can think of has been covered ... from scenery to operations to prototypes to electronics. If you missed the last few decades of National Conventions, you can still see some of the best clinics around.

Just log into www.nmra.org as a member, click on “Member / Member Home” and click on the big red box that says “Watch Clinic Videos” next to the photo. It’s that easy!

Division Reports

1st Division

Charlie Hutto, Superintendent

The First Division Board will meet on February 24 in Roseburg. This is the first time we have gotten together in a while, and we have plenty to discuss. Those items include potential hosts and locations for one or two mini-meets later this year.

2nd Division

Jeff Shultz, Superintendent

After a holiday hiatus, 2nd Division will be swinging back into every-other-month meets with one at the Wilsonville Library on February 17th at 10 a.m. Geoff Bunza will be presenting a clinic on the use of railroad telegraphy by the prototype and on the model. We'll have pizza (email jeff@shultzinfosystems.com with your pizza preferences if you'll be there). We plan to have the meet available for remote participation via Zoom: <https://us02web.zoom.us/j/87161509241?pwd=ckNvdGV0aXo0VjNwdFpBeVUzSnFwZz09>. Remote participants: Sorry, no pizza unless you get your own.

3rd Division

Shelley Shelstad, Superintendent

No report this quarter.

4th Division

Bill Messecar, MMR, Superintendent

Our seven monthly on-line clinics have resumed this year—look at the schedule in the [4th Division Grab Iron](#) and join us. Modular groups participated in shows at the Washington State History Museum Christmas attended by almost 10,000 visitors as well as the Great American Train Show in Puyallup. We have resumed our monthly virtual layout tours, which are held at 10 a.m. local time on the 2nd Saturday of the month. Join us for an interesting tour of Chuck Lee's Colorado Front Range HO layout February 10. You can also go to the [4D PNR YouTube](#) site to see any you may have missed. We presented our plan to host the 2027 NMRA National Convention in Tacoma to the NMRA BOD and hope to get their approval to proceed in the coming months.

5th Division

Bill Voogd, Superintendent

Greetings from Bill Voogd and the 5th Division. Most of us are enjoying a relatively mild and light snow winter. It certainly makes it easier to travel. My apologies to the skiers in the audience, as I know you don't share these sentiments.

In recent months, 5th Division has co-sponsored a couple of special events at the River City Modelers club in Spokane. John Langlot, a retired conductor for the Great Northern and Burlington Northern, did a very well received presentation on the GN operations between Kettle Falls, Washington, and Nelson, British Columbia. Jeff Hermann, our PNR President and 5th Division member, conducted a very comprehensive clinic on weathering rolling stock, using numerous techniques and



2nd Division Meets Online

2nd Division has scheduled Meets on the 3rd Saturday of each month. For more info, see: <http://www.2dprn.org/Events.html>

. Recorded Zoom Meets are at: <https://www.youtube.com/channel/UCWMrJ-wq0pHqoke3px4F66g>

products. It was a great hands-on learning experience for the attendees.

We're approaching the time of the year for train shows within our division. The first that I am aware of is the semi-annual Spokane Train Show, to be held March 3rd at the Spokane County Fairgrounds. This is a quite large show with 200+ vendors and several operating layouts.

We are also approaching the time for the 5th Division Annual meeting. The date is Saturday, April 20th, and the Inland Northwest Rail Museum in Reardan has graciously offered to host the event again this year. It is a fun location for this event.

We continue working on the 2025 PNR convention, to be held in early May at the Red Lion Inn in Pasco, Washington. Tours and clinics are starting to come together. If you have a clinic you would like to present, please let us know. Volunteers are always needed and if you would like to help in some capacity, contact either Bill Fassett, the convention chair, or myself.

I look forward to seeing you at future 5th Division events.

6th Division

Ed Molenkamp, Superintendent

The 6th Division is going to have a busy Spring full of shows. We're looking forward to the regional convention in Surrey and our divisional meet in June.

Membership is strong with some additional members that have become 100 percent clubs.

Join us at the first Northern Alberta Rail Modellers Meet on April 6th, 1-5 p.m. in Edmonton, Alberta. This RPM-style event will feature clinics, displays of modellers' work and works-in-progress, a show-and-share, and the chance to learn, share what you are working on, and socialize. Bring your latest models and works-in-progress! The event is in conjunction with the MMRF Spring Swap Meet at the Central Lions Senior Centre in Edmonton. Open to everyone and model railroaders of all skills and abilities are encouraged and welcomed. FREE admission with donations gratefully accepted. Central Lions Recreation Centre, 11113 113 St. Edmonton, AB April 6, 2024 1-5 p.m. The meet is sponsored by and made possible thanks to the support of the PNR 6th Division.

7th Division

Victor Gilbert, Superintendent

Plans for PNR 2024 – The Surrey Excursion are coming along nicely, and it is looking like the convention will be a good one. I am looking forward to it.

The Division has embraced the challenges of membership and is working to retain those members. We had 32 new members in 2023. January has started off nicely with four new members joining the Division.

We have an opportunity with a Vancouver secondary school (high school). Aidan Botting, an Art teacher, has started a STEAM (Science, Technology, Engineering, Arts, and Mathematics) class that is based on Model Railroading. Aidan is a new member of the 7th Division and is scheduled to present a clinic at The Surrey Excursion.

Passings:

- Norm Szun, a long-time 7th Division member, passed away on December 9th, 2023, at the age of 90. He served 25 years in the RCMP, then in BC Hydro. He enjoyed life to the fullest, especially his family, his career, fishing, designing model trains, BC history, RCMP history, aviation, and last but not least, Okanagan wine. He passed away peacefully, surrounded by his family. No service at his request.

As always I want to thank the 7th Division members who continue to deliver an exceptional experience to the model railroad community, whether virtual or in person.



4th Division Online Resources Open to All

The 4th Division has a lot of resources available on its *Grab Iron* webpage: <http://4dpcr.com/grab-iron-posts/>

These include announcements of upcoming online clinics and layout tours plus a YouTube Channel that includes recordings of past clinics and layout tours and other presentations. Here is the link: <https://www.youtube.com/c/4DPNRMovies>



NMRA Partnership Program

The partners listed below give discounts to NMRA members when orders are placed using a special discount code. Newly added partners are in **bold red**. Check them out! You can find descriptions of these companies and their products, links to their websites, and the discount codes on our Partnership Page at nmra.org/partnerships. Remember, you need to be logged into our site as a member to see the codes! Go to nmra.org/user/register to register your name on the website.

Bespoke Rail	Highways and Byways Model Railroad Graphics	Old West Scenery
CatzPaw Innovations	Hot Wire Foam Factory	R & J Details
CB Train Junction	ITLA Scale Models	Ram Track
Clever Models LLC	K.I.S.S. Method Inc	RR-CirKits, Inc
CMR Products LLC	LaBelle Woodworking Co.	Scale Model Plans
Coastmans Scenic Products	LARC Products	Scale Railroad Models
Composite Designs, Inc	Logic Rail Technologies	Scenery Solutions
Conowingo Models	MAC Rail, LLC	Showcase Miniatures
Cowcatcher Magazine	Micro-Mark	Team Track Models
Daylight Sales	Mine Mount Models, LLC	The N Scale Architect
Deepwoods Software	Miniprints	Tichy Train Group
Deluxe Materials	Model Train Catalogue	Touch of the Brush Model Weathering
Diorama Hobbies	Modelers Decals & Paint	Train Installations, LLC
Dwarvin Enterprises, Ltd.	Motrak Models	Trainmasters TV / Model Railroad Hobbyist Store
East Coast Circuits	MRC (Model Rectifier Corp)	Train Show, Inc
Enterprise & National Car Rentals	Mudd Creek Models	Trainz
Evan Designs	New Creations Victorian Railroad Buildings	TSG Multimedia
Feight Studios	Nick and Nora Designs	UGEARS
Fusion Scale Graphics	Northlandz	USA Airbrush Supply / Badger Airbrush
Gatorfoam	N Scale Works	WiFi Model Railroad LLC
Great Decals!	Old Depot Gallery	Yelton Models
Green Frog Productions		

PNR Interchange

The **PNR Interchange** is a free service for PNR members wishing to buy, sell, donate, or exchange model or prototype railroad related items. Commercial enterprise transactions and advertisements are not allowed except as provided for hobby stores below. Members may also use this service to request or solicit hobby or railroad related information, research results, and other resources or materials from other members. Members' classified ads and notices are restricted to text format only (no graphics) and length should be no longer than 120 words. Sequential ads of the same nature or having the same or nearly same content are not permitted. The suitability of submitted classified ads for publication is determined at the sole discretion of the Switchlist Editor. Email your submission along with your NMRA membership number to the Editor at switchlist@pnr-nmra.org

Hobby stores that support model railroading may include an advertisement in PNR Switchlist. The ad is limited in size to a business card size and should be provided in ready to copy and print format as determined by the Switchlist Editor. Advertisements for hobby stores that provide a discount for NMRA members shall be so identified with the advertisement. Hobby store ads may be run sequentially issue to issue.

For Sale: Various books and technical references on the **UP, SP, and B&O**, such as:

Union Pacific Lines, Common Standard Drawings, V1-V4;

SPH&TS: *Southern Pacific Passenger Cars - V1-V5;*

Southern Pacific Lines, Common Standard Drawings, V1-V5;

Southern Pacific Freight Cars, V1 - V5;

The 50 Best of B&O, Books 1 through 5.

Complete list and asking price available upon request.

For more information, please contact:

Loren Martell: saltnpapper69@live.com



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Do you have "excess" model railroad equipment, kits, or supplies?

Searching for that "special run" or hard-to-find locomotive?

Looking for prototype or historical photos, track diagrams, or other information?

Place your classified ad here in the PNR Interchange and connect with fellow PNR members!

Timetable

To add your events or activities to the Switchlist Timetable and PNR web page,, contact the Timetable Editor, Jeff Shultz (temporary) at timetable@pnr-nmra.org

Also, if you want to add your events or activities to the [NMRA.org Calendar](https://www.nmra.org/calendar), please send the event information via email to calendar@nmra.org

2024

Mar 2 (Sat), Spokane, WA

River City Modelers Open House. 1130 E. Sprague Ave., Spokane, WA. Visit the River City Western HO Layout from 4 to 8 p.m.

Mar 3 (Sun), Spokane, WA

Spokane Spring Train Show Spokane County Fair & Expo Center. 404 N. Havana St., Bldg. A, B & C. 9:30 a.m. to 3:30 p.m. 200+ tables of railroad related items for sale—model and toy trains, photos, books, toy autos and trucks, bridges and buildings. Train memorabilia – old lanterns, signs, china, linen, switch locks, keys and much more. Operating layouts in N, HO, O and Lego. Operation Lifesaver will be there with their train simulator and the Wtrak layout for kids to play with will be set up too. Free parking, food available. \$8.00 cash admission, 12 & under free. For more information contact: Shirley Sample, 509-991-2317 or via email: shirleysample13@gmail.com.

May 4-5, Kelso, WA

Longview, Kelso & Rainier Model Railroad Club Spring Train Show & Model Train and Toy Swap Meet. Three Rivers Mall, 351 Three Rivers Drive, Kelso, Washington, 98626, 10 a.m. to 4 p.m. Admission \$5, Children under 12 free with Paid Adult, Early Bird Special: get in between 9a.m. and 10a.m. for \$10.00 Each. Free Parking! 200 Tables for Vendors, On-Site Food Vendor, Vendor Info: \$25 for 8' table, bring your own cover, setup 5-9 p.m. Friday & 7-10 a.m. Saturday. For more info, visit lkr-trains.yolasite.com or contact Howard Flick at 360-751-7276 or flickh@yahoo.com.

May 22–26, Surrey, BC

2024 Pacific Northwest Region Convention "2024 Surrey Excursion" includes self-guided layout tours, contests, displays, AP evaluation, clinics, operation sessions, and in-house non-rail activities. At the Sheraton Guildford Hotel. Info: <https://pnr2024.7divpnr.ca/>

Aug 11 (Sun), Parksville, BC

Parksville Museum Railway Day. From 10 a.m. to 4 p.m. Layouts, sales tables, concessions and music. Entry by donation. Further details forthcoming by the end of March. Contact Colin Frame at frameasc@gmail.com



NOTE: Because event conditions may change rapidly, be sure to double-check the status of specific events by visiting the PNR website's list of Upcoming Events, the website of individual events, or contact the person in the specific event's listing.



NMRA Calendar Online

Are you interested in knowing what NMRA-sponsored events, other model railroading events, and train shows are scheduled beyond the PNR boundaries?

Then check out the online **NMRA Calendar**.

The calendar is available at: <https://www.nmra.org/calendar>

If you want your upcoming event posted on the NMRA calendar, email the event information to calendar@nmra.org.

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Pacific Northwest Region National Model Railroad Association

-  **Division 1:** Southwestern Oregon
-  **Division 2:** Northwestern Oregon, Southwestern Washington
-  **Division 3:** Southern Idaho, Eastern Oregon
-  **Division 4:** Western Washington, Alaska
-  **Division 5:** Eastern Washington, Northeastern Oregon, Northern Idaho, Montana
-  **Division 6:** Alberta, Saskatchewan, Northwest Territories
-  **Division 7:** British Columbia, Yukon

