



The Switchlist

Official Publication of the Pacific Northwest Region, National Model Railroad Association

May 2025 rev.



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Form 255
UNION PACIFIC RAILROAD COMPANY

SWITCH-ENGINEMEN'S DAILY REGISTER

Yard

FOREMAN	No. of Help-ers	ENGINEER AND FIREMAN	Com-pared Time	Job Work-ed	Highest Run-ner Number	Left Round-house or Side Track	SHIFT WORKED		Arrived Round-house or Side Track	HOURS WORKED		
							Began Work	Quit Work		Meal Per-iod	Straight Time	Over Time

Illinois Central Gu

Switchlist

5, Friday, February 7, 1982

at: Reserve

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Next Yard : St. Rose, LA

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Origin : Reserve, LA

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Destination: St. Rose, LA

701 Cabir: 199044

SPOT NxtLoc Tons Contents Hdg

Hall 2 42 700M 4865

Form 818-A Standard—Small
SWITCH CARS CAREFULLY AND SAFELY
AVOID ROUGH HANDLING
Santa Fe

(Insert Name of Railway Company)
SWITCH LIST

At		Engine No.		Station, 1- 3- 12		Time	
Train No.		4/20/82		6:10		M.	
350/p							
CAR							
Initials	No.	Contents	Destination	Consignee	Tonnage		
Williams	70	70	70	70	70		
68830	70	70	70	70	70		
69262	72	72	72	72	72		
68758	72	72	72	72	72		
	52	52	52	52	52		

AINSWORTH
CONNECTOR

PASCO

2025

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The Switchlist

Editor:

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406.589.6256

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The Switchlist is the official publication of the Pacific Northwest Region, National Model Railroad Association, and is published quarterly in February, May, August, and November.

SUBSCRIPTIONS

The Switchlist is available free of charge online at the PNR website, pnr.nmra.org. PNR members with current email addresses on file with the NMRA will receive an email notice when the latest issue is posted.

CONTRIBUTIONS

Readers of *The Switchlist* are encouraged to submit news, articles, photos, and drawings about PNR activities past, present and future. Articles on other aspects of model railroading are also appreciated.

To assist you with article preparation, the PNR website has posted a set of [guidelines](#) for formatting and submitting articles, photos, and other material for publication in *The Switchlist*. This also includes a style guide and list of key terms that should be helpful.

The Editor reserves the right to accept, reject, and edit contributions as space, time, and suitability of material dictate. The *Switchlist* does not pay for articles.

Please email your contributions to the editor at switchlist@pnr.nmra.org. Deadlines for submissions are:

February 1
May 1
August 1
November 1

Complete staff and contact information is available in this newsletter and on the PNR website: pnr.nmra.org.

ADVERTISING

Non-commercial classified advertising will be accepted from PNR members at no charge as a member service. Email submissions to the Editor. Hobby stores that support model railroading may also include an advertisement. See the section titled "PNR Interchange" for more details.

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Extra! Extra! Read All About It!

Those of us who have been around a few years may remember watching vintage movies and hearing newspaper boys standing on the street corner imploring folks to buy the latest edition of the newspaper. Maybe some of you are old enough to have experienced this firsthand.

Things have changed a bit since then. Now we've got digital newsletters that come delivered to your email address. And that's what we're doing here. today. Maybe you've noticed that the May *Switchlist* has been published a few weeks earlier than usual. We did this specifically so that you have the latest info about the 2025 PNR convention — [The Ainsworth Connector](#)—and this will help convince those of you not yet registered to make the commitment to attend.

So, please go right to pages 3 and 4 and "Read All About Itl.

Regional news and announcements include a progress report from President Jeff Herrmann on the PNR's emerging Modeling With the Masters® program. Jack Hamilton, MMR, HLM, shares some thoughts regarding each of us having a role and responsibility to help recruit (and retain) new members.

The PNR's Education Program chair, Lee Calkins, has recently been elected to be the NMRA's Western District Director. Congratulations, Lee! That will leave a vacancy in Lee's position. Plus, PNR Secretary Brian Orysen had to resign from his position to attend to other family needs. Please seriously consider stepping forward to help out the region in one of these positions. Contact Jeff Herrmann for more information.

The *In Memoriam* section (p.8) lists PNR members who have recently passed away. Please take a moment to remember these friends. Our sincere condolences go out to the family and friends of all these members.

Page 9 lists the PNR's newest members. Please scan the member names and their home towns. If someone lives near you, it only takes a moment to email your division superintendent to let him know you'd like to connect with this new member. Invite them to meet with your local group of modelers, or to attend your next division or club meeting, operating session, or train show. Sure, it may be a bit scary for us introverts, but the good feeling of opening the circle to a fellow model railroader is well worth it. Imagine how great they will feel!

We have five feature articles in this edition. The layout article is a special one that I think you'll really appreciate. Next up is some advice from Bob Parrish, MMR, on how to use "Prototype Modeling as a Springboard" to other Achievement Program merit awards and certificates. Larry Baxter presents the third and final part of his series of articles on JMRI OperationsPro. Bill Voogd shares another of his vacation "finds." And Bob Parrish, MMR, begins a new series of articles on freight car scratchbuilding tips and tricks.

The PNR officer, staff, and division superintendent quarterly reports begin on p.22. These folks are working hard for you and the organization, so please take a moment to catch up.

The Timetable (p.30) lists events from now through October. So, you have plenty of opportunities to get out and have some fun this Spring, Summer, and Fall.

Now that you've "read all about it," you definitely know it's not too late to register for the convention. See you in Pasco!

As always, stay safe and happy modeling!
Greg Kujawa, MMR, Editor

PNR News and Announcements

2025 PNR CONVENTION: “All Aboard!”

Bill Fassett, MMR – 2025 PNR Convention Chair

The 2025 PNR Convention—the Ainsworth Connector—is just three weeks away at the Pasco Red Lion Hotel & Conference Center from May 9 to 11, 2025, with pre-events on May 8. Updated information is available at <https://pnr5d.org/pnr-5th-division-home-page/2025-pnr-convention/>. You can also enter the following Google search terms: 2025 PNR Convention, or find the link at the [PNR Upcoming Events](#) page. The hotel room block has closed, but the Red Lion will continue to offer rooms as available.



MODELING WITH THE MASTERS®

There are still openings for the Modeling with the Masters® clinic on Thursday, May 8. Sign up during online registration.

PF&S RAILWAY SUPPLY MANUFACTURING TOUR

Phil Miller has secured approval to tour the PF&S facility. PF&S is a long-standing retail hobby store, but has also been a manufacturer of supplies for many years. They currently produce Tomar, Precision Scale, and Utah Pacific brands and are expanding to a couple of other product lines. Tour times between 10 am and 2:30 pm on Friday, May 9, are listed on the website. Each slot will accommodate 10 guests. Individuals will need to use their own cars for a 12-minute drive to PF&S, and each half-hour tour group will have its own guide. There is no charge for the tour, and a sign-up sheet will be available at the convention registration desk, first-come, first-served.

LAYOUT TOURS

Phil Miller has arranged for 11 layout tours, featuring N, HO, On3, and G scales. An “on the way” tour on Thursday from 1 pm to 4 pm features the Cambia & Black Mountain in Hermiston, Oregon. If you’re coming from southern Idaho, Oregon, or Washington along I-84, this is a great stop with about a 45-minute drive remaining to Pasco. If you think you will stop by, please go to the convention website and click on the Layout Tours tab to let us know. Private layouts in Kennewick will be open on Friday from Noon to 4 pm, and layouts in Pasco and North Richland will be open from Noon to 4 pm on Saturday. The Tri-Cities Model Railroad (TCMR) Club layout will be open on Friday, Noon to 4 pm and on Saturday from 10 am to 4 pm. All tours are self-driving.

OPERATING SESSIONS

The TCMR operating sessions will be on Thursday and Friday from 7 pm to 10-ish pm. Motive power, rolling stock, and NCE throttles will be provided, but you can use your own TCS wi-fi throttle or the Engine Driver or Wi Throttle apps on your cell phone. Indicate your interest in an op session on the online registration form.

AP EVALUATIONS AND CELEBRATION ROOM

AP evaluations will be conducted in half-hour appointments. A first-come, first-served sign-up sheet will be available at the Celebration Room check-in desk. You will need to complete the NMRA form 901 to submit your model for display or evaluation. Likewise, you may submit models for the People’s Choice Contest, for Contest Judging, or for Display-only at the Celebration Room check-in desk.

FREE-MO LAYOUTS – RUN YOUR OWN TRAIN

The module owners of both the N-scale and HO-scale Free-mo layouts have offered to allow registered attendees to operate their own N-scale or HO-scale trains

NMRA PRESIDENT TO ATTEND CONVENTION

A quick reminder that NMRA President Gordy Robinson will be attending the convention – in person.



Gordon Robinson, MMR, resides on the Orkney Islands off the North Coast of Scotland.

You now have another great reason to attend the Ainsworth Connector— to meet Gordy and also to take part in the Annual General Meeting on Saturday, May 10, at 8:00 am.

of reasonable length on the layout. A schedule of available times will be at the Free-mo room and the convention desk. Bring your own favorite power and rolling stock and take part!

MOMS – PICK UP YOUR GIFT

All moms attending the convention are invited to pick up a special gift at the convention desk.

ANNUAL GENERAL MEETING

The Annual General Meeting will be held on Saturday morning at 8:00 am, featuring NMRA President Gordy Robinson live and unplugged! As in the past, we will make this meeting available outside of the convention on Zoom. Watch your inbox for an announcement of the Zoom meeting link.

CONVENTION SCHEDULE

Thursday, May 8

- All day: Modeling with the Masters® – sign up during on-line registration
- 1 pm–4 pm: “On the Way” layout tour – Cambia & Black Mountain RR, Hermiston, Oregon
- Afternoon: Free-mo setup
- Late afternoon: sign-in at hotel
- 7 pm–10 pm: Operating Session at Tri-Cities Model Railroaders (first-come, first-served based on interest expressed during online registration)

Friday, May 9

- Sign-in at hotel – all day
- Plus One room open
- Celebration Room open – model check-in
- Free-mo layouts open
- PF&S Manufacturing Prototype Tours: 10 am–2:30 pm, every 1/2 hour (sign-up sheets at convention desk)
- Clinics
- Layout tours: Noon–4 pm
- Afternoon: AP evaluations
- 5 pm–6 pm: Mix, Munch, and Mingle buffet and no-host bar
- 7 pm–10 pm: Operating session at Tri-Cities Model Railroaders (first-come, first-served based on interest expressed during online registration)

Saturday, May 10

- 8 am – PNR Annual General Meeting
- Plus One room open
- Celebration Room open
- Free-mo layouts open
- Clinics
- Mini-Clinics: 7–9 pm
- AP evaluations
- Layout tours: Noon–4 pm
- Evening: Contest judging

Sunday, May 11

- Plus One room open
- Awards displayed in Celebration Room
- Morning: Clinics
- 10 am: deadline to pick up models from Celebration Room
- Noon: Awards Luncheon with special appreciation for mothers present

FOR MORE INFORMATION

Convention Chair – Bill Fassett, MMR, b.fassett@comcast.net; 509-979-5205

Clinic Chair – Ron Edgar, redgar53@gmail.com; put “Ainsworth Connector” in subject line

Convention Registrar – Lori Sebelley, PNR2025Registrar@outlook.com

MODELING WITH THE MASTERS® NOT JUST FOR CONVENTIONS ANYMORE!

Jeff Herrmann, MMR, PNR President,

For some time now, the only way to participate in a *Modeling With The Masters*® experience has been to spend an extra day or two at a national or regional NMRA convention. That is beginning to change in PNR!

The PNR Board of Directors believes that *Modeling With The Masters* (MWTM) is a benefit of membership and should be available to all members and we should take the NMRA's *Modeling With The Masters* program on the road around our region and make it more convenient and less expensive for our members to participate than the current plan offered nationally. Having a team of "certified instructors" in each Division will help us reach this goal. NMRA has given PNR permission to create a "certification process" for MWTM instructors. The Region program must satisfy the quality and program objectives of the National program. The Region effort is a part of and not separate from NMRA MWTM.



The classroom setting in Kennewick.

On Saturday, March 1st, we held the first MWTM event of this kind in Kennewick, Washington (5th Division). There were ten participants in the session and we were able to "certify" eight MMRs as instructors. The session used two "challenges" to help the participants increase or enhance their modeling skills, introduce them to new modeling materials and techniques, and introduce them to various tools used in the modeling process. The NMRA Achievement Program (AP) sets forth a number of qualities and characteristics demonstrated by a good model builder. The same qualities and characteristics are incorporated in the selection of a specific challenge for each MWTM activity. In the Kennewick session, participants learned how to build jigs for scratchbuilding projects and how to construct a wood tunnel portal with interior detail. While it is not a primary objective of MWTM, five merit awards were presented for the tunnel portals at the end of the session.



Our fearless leader, Jack Hamilton, MMR, HLM.



Five of the ten participants learning new model building skills. Left to right: Cory Bennet, Lee Chessman, John Christner, James Keena, and Bill Voogd.

MWTM sessions will next be held at the PNR Convention in Pasco on May 8th, and in the Tacoma area and Canada, hopefully by year-end!



Here's one of the completed "challenges" — a scratchbuilt tunnel portal.

PNR Annual Business Meeting to be Held Saturday, May 10th

Jeff Herrmann, MMR, PNR President,

The PNR Annual Business Meeting will be held on Saturday, May 10, 2025, at 8:00 a.m. Pacific Time at the Red Lion Hotel in Pasco, Washington, in conjunction with the PNR Convention. The meeting will last approximately one hour and the agenda will include:

1. Regular Business: Secretary, Office Manager, and Treasurer reports
2. Comments from Gordy Robinson, MMR, NMRA President
3. Update on Achievement Program
4. Comments from 2025 Convention Chair
5. Announcement of 2026 PNR Convention
6. 2027 NMRA Convention in Tacoma, Washington

The meeting will also be available via ZOOM. A ZOOM meeting invitation with passcodes will be sent to PNR membership via email in advance of the meeting for those unable to attend in person.

That's Not My Job! — Says Who?

Jack Hamilton, MMR, HLM, 4th Division

“Why do you guys at the Region and National always make such a big issue about membership numbers?” “We have almost fifteen thousand members and my club is doing just fine since we don’t go on the road anymore.” “We don’t need those young guys to set up the layout anymore.” “There can’t be much of a problem if all the train shows are doing so well.” “How about we give it a rest and go back to having fun?”

Those are the standard responses to calls for attention to Association recruiting and retention. In summary, it’s just too hard to exchange time operating or running trains to any effort to find or help assimilate a new member. Is that the way you became and remained a member? Nobody cared??

The power of our Association is in numbers. We are a volunteer organization that relies on the good offices of a few members to reach out and deal with the industry. When we represent a significant part of the consumer base for the model railroad industry, we have a strong voice. Without strong membership numbers we will find it increasingly difficult to get major manufacturer participation in activities sponsored in part or in whole by NMRA. When we cease to be an effective test bed for new ideas and new products, we will also lose the ability to make standards applicable. Our relationship with the hobby industry works both ways, so long as both sides are actually representative. We need numbers.

There is never any shortage of complaints when an increase in annual dues is proposed. While those increases are primarily the result of normal increases in the cost of living, membership numbers also play a part in the Association finances. The NMRA Headquarters (and the Association in general) operates with a number of basic costs. Staff salary and benefits, rents and fees for the building and equipment, software licenses, and similar don’t care how many members they are serving— they just eat up the cash. When membership numbers fall, a greater percentage of each member’s annual dues is shaved off to cover the standard operating costs. The simplest way to avoid increases in annual dues is to significantly increase membership.

My final consideration is that guys get old and sometimes move away. In a fixed-numbers Association, we would be fortunate to cycle and recycle the current member pool so that we are all able to maintain a reasonable “Fellowship” level in our Divisions and local groups. Unfortunately, as recent history shows, when numbers decline, the local impact can be significant. If the guy who always did something to make things work gets transferred or becomes a “snow bunny,” the world seems to have just gotten a lot smaller quickly. It was great to “Let George do it” when he was around, but now that he is gone, we find out we needed a backup plan. The idea that we are all a bunch of Lone Wolf hermits living in single-purpose man caves may merit a chuckle or two but it does not reflect a condition we can thrive under.

The bottom line of all this preachy stuff is simple. Each and every member of NMRA is responsible for the recruitment of new members and the retention of existing members. There is no “That’s not my job.” because it is. If we want NMRA to be there for us in the future and if we want future generations to enjoy this hobby as much as we have, we need to accept our responsibility and get to work. NMRA is a Portal to Fun only if we can bring more modelers through that portal.

Lee Calkins (5th Division) Elected as NMRA Western District Director

Lee Calkins, PNR Education Committee Chair and member of the 5th Division, has been elected to be the NMRA's new Western District Director. Below is some background that Lee provided about himself.

My name is Lee Calkins. I live in beautiful North Idaho. I have a wife and four kids. I model the Union Pacific and Milwaukee Road in Idaho in the 70s and 80s. I am a civil engineer by trade. I believe Families are essential in the grand scheme of things, and that reaching out to them about Model Railroading and the NMRA is key to sustaining and growth of the hobby and the organization.

My philosophy that I have shared for many years with the Free-moN community is "People are more important than trains." I believe that Family is of the highest importance and that by bringing Families into model railroading is key to the Growth of Model Railroading. I also believe people need good "Family Friendly" activities, a place to connect with others in the hobby in a fun and enjoyable way.



Right: QR code to Lee's Facebook page.

Not getting the bi-monthly NMRA eBULLETIN?

The new **NMRA eBulletin** comes out every other month near the first of the month. Either your SPAM filter is blocking it, or you need to update your email address with us. Have HQ update your address at: <http://www.nmra.org/customer-service-request>



PNR Constant Contact Resubscribe

Any member who has elected to unsubscribe to the PNR Constant Contact direct email may resubscribe, but Constant Contact only allows this to be done once for each member.

Here are the instructions to resubscribe:

1. Send an email to your division superintendent with your name, NMRA #, email address, and a request to resubscribe.
2. Division superintendents will send this information to the PNR Office Manager who will then send the resubscribe Constant Contact request to the member.

In Memoriam

This section is dedicated to the remembrance of those members of the NMRA Pacific Northwest Region whom we have lost over the past year. We extend our deepest sympathies, thoughts, and prayers to their families and friends. We will sincerely miss the fellowship of these folks and all their contributions to the NMRA and hobby.

Deceased Members of the PNR — April 2024 through March 2025

Month Reported	Div	Last Name	First Name	City/Town	State/Province	Special Recognition
April 2024	1	Bain	William	Newport	OR	Life Member
May	3	Roderick	Wayne	Pocatello	ID	Life Member
June 2024	2	Auburg	Doug	Battle Ground	WA	Life Member
July 2024	6	Bruneau	John	Camrose	AB	
	4	Liesse	Ed	Federal Way	WA	Life Member
August 2024	4	Mundy	Douglas	Gig Harbor	WA	
	4	Hamilton	Fred	Maple Valley	WA	
	4	Black	John	Mount Vernon	WA	
September 2024	7	Sparks	C D	Burnaby	BC	Life Member
October 2024		None				
November 2024	4	Moorman	Jeffrey	Ferndale	WA	Life Member
	4	Vaughn	Paul	Graham	WA	Life Member
December 2024	1	Crueger	Jim	Monroe	OR	
January 2025	2	Kaser	Fred	Molalla	OR	
	6	Bryce	William	Lloydminster	AB	Life Member
	6	Johnson	Mark	Edmonton	AB	Life Member
February 2025		None				
March 2025		None				

**Have you changed your address or other membership information?
Notify NMRA Headquarters**
 email: hq@nmra.org
 Phone: 423-892-2846 (8 a.m.–4 p.m. ET)
 Mail: P.O. Box 1328, Soddy Daisy TN 37384-1328

New PNR Members

February 2025–March 2025

We extend a warm welcome to all new members of the Pacific Northwest Region, NMRA. Those listed below have joined the PNR over the past quarter. If you do not see your name listed, please be patient. Your membership is likely still being processed.

We are especially happy to welcome our new Rail Pass members (*identified in blue text*). We recognize that you are with us on a trial basis and we will have to work hard to convince you to stay with us. Please make sure to contact your Division Superintendent promptly (listing at the end of this newsletter) so you can be included in all Division activities.

Current Members: Please review this list for anyone living near you and consider initiating a connection with them through your Division Superintendent.

Div	Last Name	First Name	City/Town	State/Prov	Div	Last Name	First Name	City/Town	State/Prov
01	Winke	Dale	Eugene	OR	05	McClung	Warren	Spokane	WA
02	Pickett	Tom	Portland	OR	05	McElwain	Rod	Bozeman	MT
03	Feuerborn	Richard	Boise	ID	05	Kennedy	Christopher	Post Falls	ID
04	Watson	Paige	Snohomish	WA	05	Herrmann	Benjamin	Pullman	WA
04	Mckenzie	John	Camano Island	WA	05	Waldron	Janet	Deer Park	WA
04	Green	Charity	Federal Way	WA	06	Boote	John	Calgary	AB
04	Murray	Aidan	Seattle	WA	06	Boote	Kieran	Calgary	AB
04	Jordan	Jerry	Anchorage	AK	07	Mohr	Mike	Garibaldi Highlands	BC
04	Terpstra	Toren	Kirkland	WA					



Attention All PNR Members! Have you taken the PNR Survey yet?

Even though you may no longer consider yourself a “new” member of the National Model Railroad Association, we need your input to help us develop and plan programs, events, and activities that meet your needs as a member of the NMRA in your area. Please take just a couple of minutes to fill out our on-line survey at:

<https://www.surveymonkey.com/r/K98F93Q>

Thank you in advance for participating in this survey to help the PNR deliver benefits to you!

Features

Editor's Note: In the February 2022 Switchlist, we began running a series of articles featuring PNR members' layouts and modules, and this also includes PNR club layouts. The intent of this series is to have an easy way to share your ideas, vision, modeling interests, and progress across the vast distances of the PNR. These articles can be as simple or detailed as you want. You can review past issues of the newsletter to see what your fellow PNR members have submitted.

Please consider submitting an article about your home or club layout or module.

No Toys Allowed on My Layout

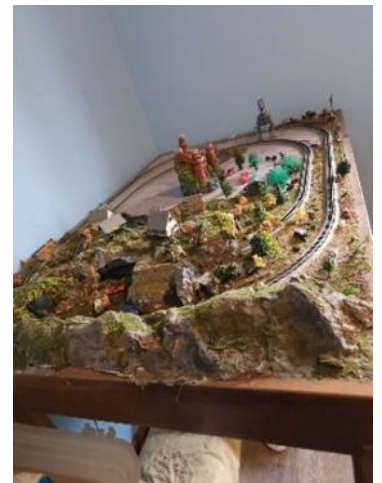
Article by Jeff Herrmann, MMR, 5th Division

Photos by Jessica Josephson

Meet 7-year-old Ryker Josephson of Post Falls, Idaho (5th Division), who is not only the youngest NMRA member in the Pacific Northwest Region, but also the youngest in the entire NMRA!

Ryker was signed up as an NMRA member on his fourth birthday by his grandfather, Lowell Didas, who was serving as the Region President of NMRA's Rocky Mountain Region at the time.

Ryker reports that, when he was two years old, his grandfather bought him his first Lionel train set. When Ryker was four, he and his grandfather built his N scale layout and he has been working on scenery ever since. Ryker insists that "No toys are allowed on my layout!"



NMRA Model Railroad Directory

Now, whether you're at home or traveling, you can contact other NMRA members for a layout tour, to talk trains, or even to join in an op session! There are maps, descriptions, photos, videos, and more. And it's only available to NMRA members.

Visit www.nmra.org, click on "Member Home," then "Directory" to see layouts in your area or to list your own layout (as long as it's an operating home or garden railroad). Join in the fun!

Prototype Modeling as a Springboard

Article by Bob Parrish, MMR, 3rd Division

Editor's Note: Astute readers of The Switchlist may notice that this article sounds vaguely familiar. We did run it in the February 2025 newsletter as a sidebar at the end of the Achievement Program report. We thought it valuable enough to the Achievement Program to update and re-run it in this issue as a stand-alone article.

There are any number of interests in the railroad modeling world. Also, as learning styles vary, so do construction styles. Some modelers dig in and work on a kit or project from start to finish with few interruptions. Others will have a number of things on their workbench which may include completely different areas of modeling. This speaks to the differences in people.

There is a wide range of opportunities from the previously mentioned highly focused modeler to the scattered attention span of people like myself. This has an impact on how a modeler may proceed in their entry into their Achievement Program (AP) quest; that is, whether they start in one area and stay with it until completion of a certificate in structures, for example, or dabble in several modeling areas concurrently. Either way is thoroughly acceptable.

When previously asked by a modeler where I might suggest that they make their start, my reply was usually to first question where their interests lie and then offer suggestions.

In a recent conversation with Duane Richardson, AP manager for the Lone Star Region of the NMRA, the Prototype Models certificate came up. He reminded me that much of the modeling in this area may also be used toward other AP merit awards and certificates. In this conversation, it was also noted that Prototype Models is probably the least pursued certificate of the eleven possibilities on the AP trail.

He stated that he had adjusted his thinking on this topic and shared a story of guiding a nephew into the AP. He spoke of a direction that I had long thought about but with no great ability to articulate until now. That is, “The Prototype Models certificate really can be the springboard to many other categories.”

A quick reading of the rubrics for this category outlines the necessary components that must be modeled from a prototype scene or a historical photograph of a scene. Those elements include: motive power, rolling stock, structures, and associated scenery. Reading further will reveal that any of these components may be further evaluated for merit awards and recognized in those specific modeling areas.

Also, pursuit of the Prototype Models certificate may become an additional gateway towards the Civil Engineering category and contribute to the square footage count in the Scenery category.

This revising of my thinking has now brought me to what now seems to be a better response for the probing question of an AP starting point. The Prototype Models certificate truly is a springboard for a foray into the hobby. A modeler may still home in on those things of particular interest; but, in the end, this category opens doors wide to the possibilities of this hobby.

Please consider the Prototype Models certificate when plotting your own AP course or in conversations with other modelers regarding their plan of pursuit.

Bob Parrish and I will be presenting a clinic on the Prototype Models certificate at the Ainsworth Convention in Pasco.—Ed.

JMRI OperationsPro Features and Hints—Part 3 Trains and Schedules

Article and image by Larry Baxter, 5th Division

Editor's Note: This is the third and final part of a series of articles discussing JMRI's OperationsPro software. Part 1 was published in the August 2024 Switchlist and Part 2 in the November 2024 Switchlist.

Part 3—Trains and Schedules

Picking up where we left off last November, we'll need to first get back into JMRI OperationsPro. To do this, open JMRI PanelPro, then navigate through the menu under Tools/OperationsPro/Trains.

Trains. A train can be added using the “Add Train” button at the bottom of the “Trains” dialog box. Give it a name or number, add a description, departure time, and select a route.



The car types handled can be selected after adding the train. A train can be edited by selecting the “Edit” button to the right of the listed train.

Tools. Select “Tools” at the upper left-hand corner of the Edit Train dialog box to manage how the train is built. The “Allow Local Moves ...” permits the train to move cars within a location. This is needed when employing Alternate Tracks. I uncheck the “Allow Cars to Travel from Origin to Terminal” for turn-around trains. This prevents hauling cars around, just to move them from one track to another.

Building Trains. Building a train generates a manifest (work order). See the example below. To build a train, select “OperationsPro/Trains,” press the “Build” button for the desired train. To build several trains, mark the check boxes on the left, press the “Build” button at the bottom center of the page. The trains will be built in departure order. The manifest can

Philmont Turn (Eastbound)

- 1 -

March 18, 2024, 1:24 PM

Start Time:

End Time:

Operator:



Manifest for train (290) Philmont Turn (Eastbound)

Type: branchline local Difficulty: difficult Level: lower level

Scheduled work at Cedar Springs, departure time 10:15

[]	Pick up	SP	401927	C4	Gray from "Track-1"	-> Philmont
[]	Pick up	RWGX	360699	WC	Yellow from "Track-1"	-> Philmont
[]	Pick up	LK&R	16082	WC	Brown from "Track-1"	-> Philmont
[]	Pick up	SP	227017	B3	Brown from "Track-1"	-> Philmont

Train departs Cedar Springs Eastbound with 4 cars, 237 feet, 322 tons

No work at Hope Springs

Scheduled work at Philmont, arrival time 10:23

[]	Pick up	UP	90490	C6	Gray from "Chemical-1 spot 2"	-> Hope Springs
[]	Pick up	SI	4908	A2	Brown from "Chemical-1 spot 2"	-> Hope Springs
[]	Pick up	WP	20807	B2	Brown from "Chemical-2 spot 2"	-> Hope Springs
[]	Pick up	MILW	4176	B9	Yellow from "Paper Mill"	-> Hope Springs
[]	Pick up	SSW	62699	B9	Brown from "Paper Mill"	-> Cedar Springs
[]	Pick up	UP	147843	WC	Brown from "Chip-1"	-> Cedar Springs
[]	Pick up	UP	147825	WC	Tuscan from "Chip-1"	-> Cedar Springs
[]	Set out	SP	401927	C4	Gray to "Chemical-1 spot 2"	
[]	Set out	RWGX	360699	WC	Yellow to "Chip-1"	
[]	Set out	LK&R	16082	WC	Brown to "Chip-1"	
[]	Set out	SP	227017	B3	Brown to "Paper Mill"	

[] Local Move MP 269252 B3 Brown from "Storage"
-> Philmont, "Paper Mill"

[] Local Move NS 450769 B3 Brown from "Storage"
-> Philmont, "Paper Mill"

Train departs Philmont Westbound with 7 cars, 418 feet, 476 tons

Scheduled work at Hope Springs, arrival time 11:12

[]	Set out	UP	90490	C6	Gray to "Storage"
[]	Set out	SI	4908	A2	Brown to "Storage"
[]	Set out	WP	20807	B2	Brown to "Storage"
[]	Set out	MILW	4176	B9	Yellow to "Storage"

Train departs Hope Springs Westbound with 3 cars, 196 feet, 173 tons

Scheduled work at Cedar Springs, arrival time 11:28

set out cars that don't fit in 1 track.

[]	Set out	SSW	62699	B9	Brown to "Track-2"
[]	Set out	UP	147843	WC	Brown to "Track-3"
[]	Set out	UP	147825	WC	Tuscan to "Track-3"

Train terminates in Cedar Springs

Example of a Manifest

be previewed or printed by selecting the appropriate button at the bottom of the dialog box. The Automation feature allows building the trains in a different order. Automation is found in the “Tools” button in the upper left-hand corner.

Switch Lists. Switch lists can be generated for a location. It shows all pick ups and set outs to be made there.

Terminate vs. Reset. “Terminate” or “Reset” can be selected in the “Action” area at the bottom right of the Trains dialog page. The selected function will show on the action buttons at the far right of each train.

- **Terminate** a train when the work has been completed. This will move the cars in the computer.
- **Reset** a train when it has not been run, and it is desired to rebuilt it, or build other trains using those cars. The cars will not be moved in the computer.

Do not press the wrong button!

Spur Capacity. JMRI makes pick ups first, then set outs when working a spur. This makes room for inbound cars. Having only one job working a particular spur prevents most problems. When more than one train is scheduled to work a spur, congestion can occur if the train setting out arrives before the train picking up. Planned pick ups will encourage JMRI to forward cars to the full track, but not prevent overfilling. A message can be added to both manifests to pull, spot, and leave cars not taken where convenient.

Managing Car Routing. The simplest way to control the flow of cars is to restrict spurs to receive only L or E loads. Example: The gravel pit receives only hoppers loaded E and the ready-mix plant only receives hoppers loaded L.

Schedules. Schedules in JMRI are similar to railroad waybills. They route individual cars from spurs to various destinations, sometimes with custom commodities. Schedules are not to be confused with train schedules, which JMRI calls routes. Schedules can only be applied to spurs, not yard, classification/interchange, or staging tracks. They specify:

- Car type
- Random (less than daily factor)
- Delivery (days spur receives this car type)
- Road (can be restricted to a specific reporting mark like BNSF)
- Receive (load type: E or L or custom designated load like “sand”)
- Ship (load type)
- Destination (location)
- Track (spur)
- Pick-up (days of the week car is picked up)

Only the car type is a mandatory entry. If the other fields are left blank, JMRI will move the cars unscheduled like it does normally. Both the shipping and receiving spur need to have a schedule when custom loads are used.

An example of a simple schedule is the log reload at Highland receives log cars Empty and ships logs to the dump at Lone Pine. The Lone Pine log dump has a schedule that receives log cars with load logs, and sends them to the reload Empty. This captive service could also be simulated using Return When Empty (RWE) and Return When Loaded (RWL) options for each log car. Alternatively, the schedule for the reload at Highland could ship log cars with load L to both Lone Pine and another mill at Keystone, with the cars designated RWE to Highland.

Here is an example of a more complicated schedule: The Meat Packing Plant ships hides in dedicated boxcars to a Tannery which receives hides and releases the cars RWE to the packing plant. The Tannery also receives tank cars of chemicals and free-running empty boxcars. The boxcars are loaded with leather and shipped to a Leatherworks. The Leatherworks reloads some of the boxcars L (representing shoes, etc.) and ships them to various distribution warehouses, and releases the rest E (empty). These moves are all less than daily, but there is an alternate track for the Tannery to hold excess cars until there is room on spot.

Custom loads can be used to classify cars. As an example: Empty insulated boxcars (BI) arriving at Easton yard are set out on track 8, the only track accepting BI empties. This is a C/I track for the River City switcher. That job pulls BI loads, and spots BI empties at the brewery. The brewery ships BIs loaded with beer to the grocery outlet at Columbia Heights, and cars loaded L to the grocery outlet at Weston. Beer is only accepted at Easton by track 6, where it is picked up by the train handling cars for Columbia Heights. The L loads are only accepted by track 5, where it is picked up by the train handling cars destined to Weston.

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Bespoke Rail	Klawndyke	Ram Track
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Clever Models LLC	Leavin' A Mark Engraving	Scale Railroad Models
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Deepwoods Software	Miniprints	Third & Townsend Models
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Diorama Hobbies	Model Train Catalogue	Touch of the Brush Model Weathering
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ITLA Scale Models		

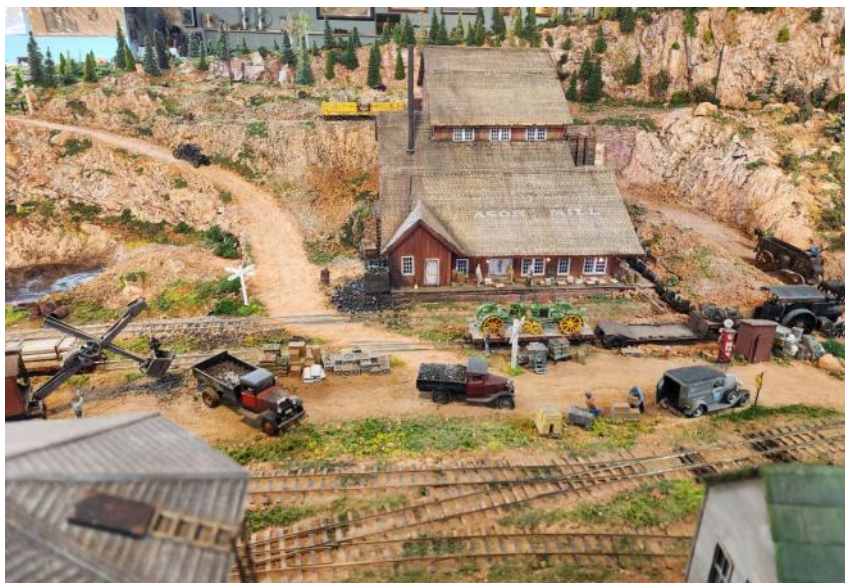
Vacation Treat

Article and photos by Bill Voogd, 5th Division

Whenever I travel to other areas, I always check for model train stores and club layouts in the area. I recently vacationed to Port St. Lucie, Florida. My brother-in-law has a friend who knew this guy with a pretty nice model train layout in his basement and who loved to show it to visitors. Of course, I said yes, but really had no idea what to expect.

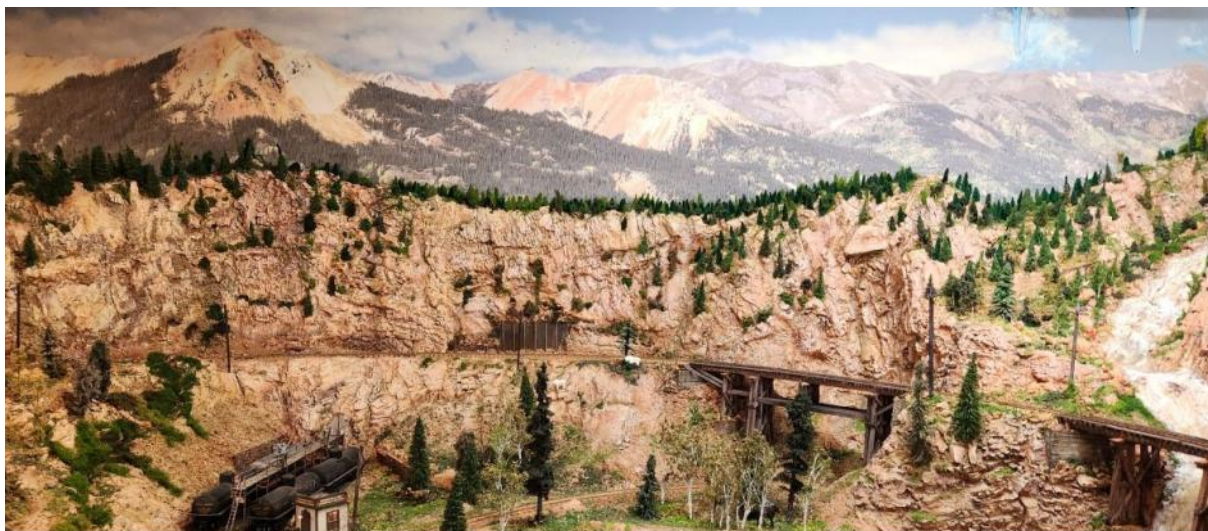
Much to my surprise, I was treated to the extremely high-quality home layout of Al Sohl (MMR #305). His Western Bay Railroad layout was just amazing ... and he was a fabulous host. The On30 layout is set in the mountains of Colorado in the year 1939 and seeks to duplicate narrow gauge operations in that area. The railroad models the operations of the Denver & Rio Grande and the Rio Grand Southern, primarily focusing on the movement of mining products, cattle, and other local industries. The narrow-gauge carloads are moved to a terminal yard where they are transloaded to standard gauge cars for shipment to Denver and beyond.

He started work on the railroad when he retired to Florida in 2010, had his track-laying golden spike ceremony in 2012, and has been working since then on completing the amazing scenery and other features. The 950 square-foot space available is utilized well and split into two divisions for operations. The buildings are a combination of kits and scratch-built and most have fully detailed interiors. The roadbed was built using the Masonite spline method. To complete the rest of the layout, Al has a very unique process of doing extremely lightweight molded terrain pieces which readily lift out to give easy access to wiring, switch motors, etc. It makes layout maintenance very easy. Train control is accomplished through DCC and operations are a modified version of the JMRI operations program.



The scenery includes some of the most beautiful rock work I've ever seen. Ground cover and trees are very realistic looking. The buildings are beautifully set into the terrain. Lots of period-correct vehicles, people, and the other little details that make the layout come to life—there are things happening everywhere. I've attached a few pictures, but check out his website at www.westernbayrailroad.org. It definitely gave me inspiration to start turning my paper plans and ideas into an actual layout.

So when you are travelling this summer, don't forget to research the internet for any model railroads (or train stores) in the area you are visiting, and include that in your vacation itinerary. It is a real vacation bonus.





Trick Shots for Building Freight Cars—Part 1

Article and photos (except where noted) by Bob Parrish, MMR, 3rd Division

Editor's Note: This is the first part of a series of articles by Bob Parrish, MMR. Subsequent parts will be published in future issues of The Switchlist. Bob provides additional advice that is posted as free downloadable PDF files on the LaBelle Woodworking website. These files are under the tab labeled [8-Hints and Pointers](#).

Part 1—The Basic Box

Introduction. Some time ago, I built a batch of wood boxcars that fit my time era, 1897-1914. I am going to walk you through some steps and what I call *trick shots* of keeping stuff straight and square. With that also, some of these ideas may help you receive an Achievement Program (AP) merit award. Please note that this series of articles is not intended to walk you through constructing a car from start to finish, but rather to provide supplemental instructions, construction tips, and alternative approaches when scratchbuilding older era freight cars.

I will begin with techniques that are relevant to all basic boxcars and refrigerator cars. There will be a final part which will address the construction of flat cars and gondolas. In that, I will deal with open floors and decks of flat cars and gondolas.

The wood materials I have used are from Northeastern Scale Lumber Co. and the plastic detail stuff and grab irons are from Tichy Train Group. Both companies are great for turnaround times on orders.

Over the years, I have built so many Labelle Woodworking and Central Valley Model Works cars that I no longer need comprehensive drawings. Figure 1 is the end view of a nineteenth century box car that was taken from the book *The American Railroad Freight Car*, by John White. Whether you are building for an award or just knocking around your railroad, this or a comparable drawing is the starting point for scratchbuilding freight cars.

You will need a prototype to work from and suitable dimensions. The dimensions are of your choice. But be aware that, to get the final height and length of car you want, the dimensions of the basic box (or core) will need to be reduced by the thickness of the scribed panels which will subsequently be attached to the core. To look at this from another perspective, the side and end dimensions of the core will need to be increased by two thicknesses of the siding material and one thickness for the car roof. For example, the height up to the roof peak will be affected by the thickness of your roofing material, so short-

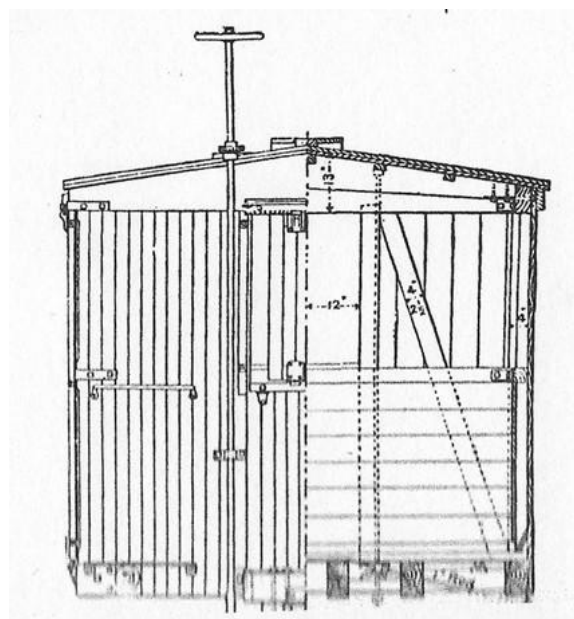


Fig. 1. Starting plan for end detail. 1883 Great Northern box car. The sub floor transverse tie rods across the car above the truck bolsters can be seen on the lower right of the plan. Source: *The American Railroad Freight Car*, by John White.

en the height of your end blocks to adjust for the added roofing material. As mentioned earlier, the total height dimension is certainly a decision that must be made at the outset of construction.

The story and photos shown in this article will yield a 38-foot car at 8-1/2 feet wide.

The core wood of a boxcar or reefer includes four parts: roof board, floor board, and two end blocks. **Be sure that the roof and floor board are exactly the same length. Similarly, be sure that the end blocks match exactly in all dimensions.** If the core frame of your boxcar is not perfectly square when glued, nothing thereafter will be correct. We will delve into this in more detail later.

Floor. If you are not building these cars for the AP, then a solid block floor may be used (shown in Fig. 2). This part may be ordered from Northeastern Wood Products in either 12- or 24-inch lengths. Be careful of the scale you are modeling when placing the order. You may choose to skip the next steps on framing up the floor.

If you decide to use a solid wood block for the floor, it's easier to mark locations for the truck bolsters, truss rod string

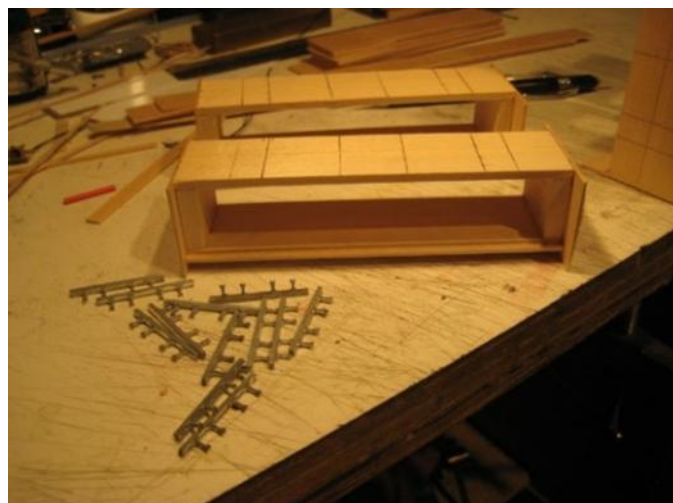


Fig. 2. The basic box. This is the starting point for boxcars and refrigerator cars. The dimensions of this box will establish the size of the car.

This photo shows a solid wood floor facing up. Note the pencil lines on the solid wood floor, which is facing up. The pencil lines are the locations in order from left to right for: truck bolster, truss rod string holes, queen posts, queen posts, string holes, truck bolster.



Fig. 3. I use a machinist's square clamped to the edge of my bench to get everything square.



Fig. 4. Here are the center sills, stringers, side sills, and end sills laid out on top of the floor made from scribed wood siding. The floor has not been cut to length yet.

holes, and queen posts prior to assembling the box (Fig. 2).

If this is to be an AP merit award candidate, I suggest you build the floor with an underframe and flooring that will aid in scoring as it increases the complexity of the model.

For this option, cut the center sills, side sills, end sills, and stringers accordingly. Often the two center sills were taller than the two or three stringers and side sills on each side of center. I am doing this with this floor structure (Figs. 3 and 4).

Next, you have a choice. You can hand lay individual boards that will show through to the bottom or use scribed siding facing down as I did here (Fig. 4).

Bolsters and Transverse Truss Rods. In the 1883 end view plan shown earlier, a transverse truss rod can be seen (faintly) that ran from side to side over the truck bolster and center sills. This rod reduced sagging from side to side. It is the same logic that was used on longitudinal truss rods to stop center car sagging. These rods are usually a forgotten piece of modeling freight cars of this period, but when going for a merit award, this needs to be considered. Evaluators will probably not look for this and won't ding you for leaving it out. Judges are to look at what you present them and how well you did it

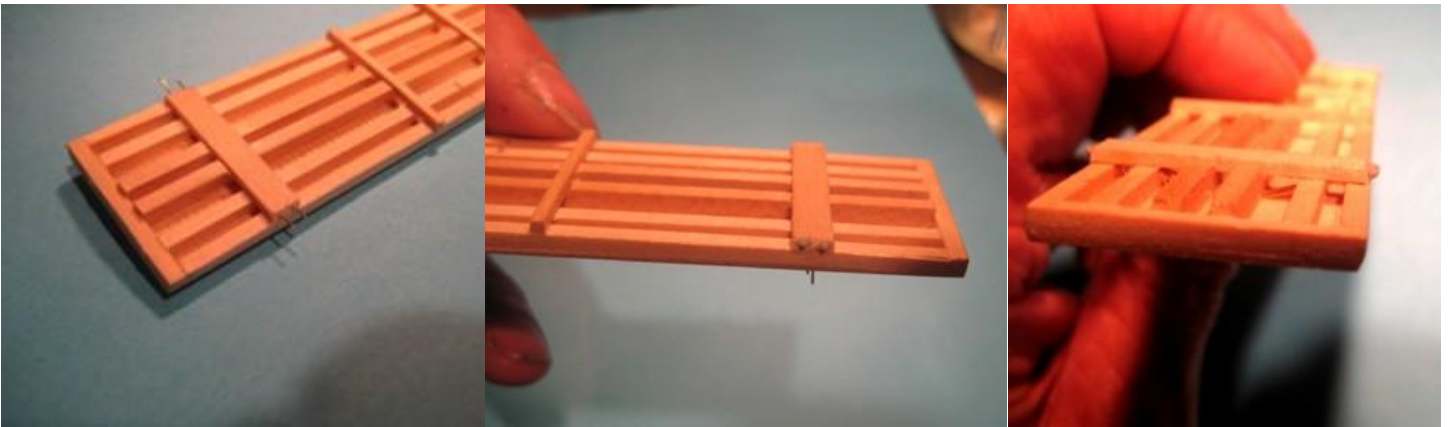


Fig. 6. *Left: Showing the bolster drilled and wires exposed. Center: A side view of the NBWs in the bolster ends. Right: Looking in from the end you can see the transverse truss rod wires and how they align with the NBWs.*



Fig. 7. *This shows the wires installed before the NBWs are installed.*

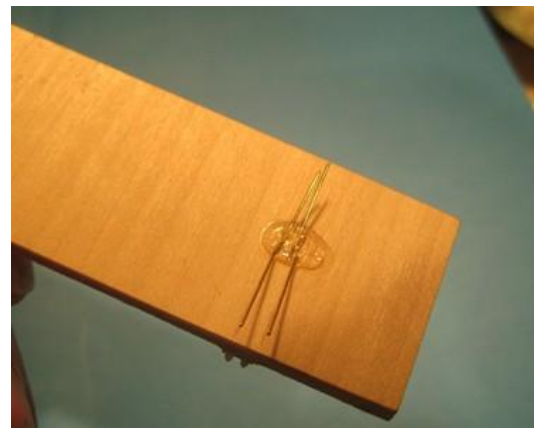


Fig. 8. *The tag ends of the wires with the NBWs installed. Secure the wires with a dab of CA glue.*

rather than what THEY think ought to be there. However, when preparing the paperwork, noting this detail will aid in locating the model in the evaluation matrix for complexity, conformity, and detail. Be sure to point out this detail because much of the evaluations in the PNR and some divisions are conducted in a conversational format with the modeler.

I am building the bolsters with NBW (nut-bolt-washer) castings from Grandt Line and the transverse truss rods are 0.014" brass wire. For this project, I ran the wires out the top of the floor as it is a boxcar and what is inside goes unseen.

Figures 6-8 show the drilled holes and wires that go through the bolster and stringer and come out above the deck over the center beam for that side. I don't glue them in as yet due to the space that the NBWs will take up and to make the wire look like it is in line with the NBWs.

Longitudinal Truss Rods. There is another choice to be made here and that is of the material for the longitudinal truss rods. For AP models, you may want to use 0.014" brass wire and run them end to end through the end beams. If you decide to use monofilament fishing line, that will be discussed shortly.

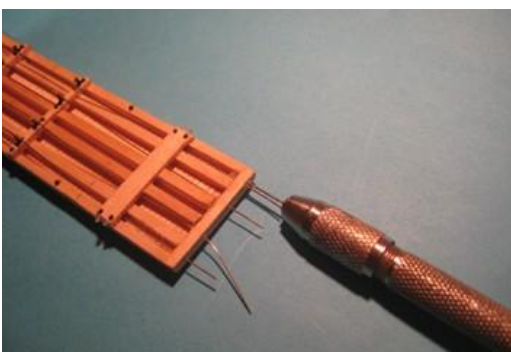


Fig. 9. *Drilling holes through the end sill for truss rods.*

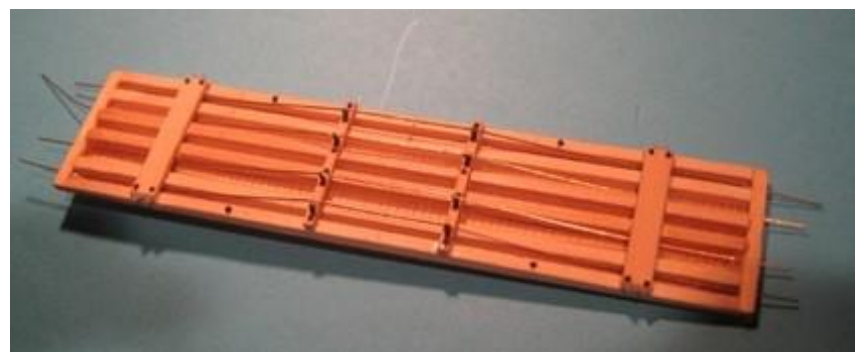


Fig. 10. *Here is a look at the underframe with all of the NBWs, queen posts, and untrimmed wire for truss rods. The queen posts are four-inch lift from Grandt Line. San Juan Models has acquired Grandt Line, so these are still available.*

Notice when I installed the wires for the truss rods (0.014" wire), I left the ends sticking out (Figs. 9 and 10). It's a bit wasteful of wire but it gets you to the next step. When installing the NBWs on the end sills, it helps to put the casting in line with the actual wire. Evaluators will see this subtle piece. The wires are nipped off after the drilling process. Then the NBW castings can be inserted. For boxcars and reefers with wood siding, you will want to install the NBWs after the siding is glued to the end blocks, as I did in Figure 12.

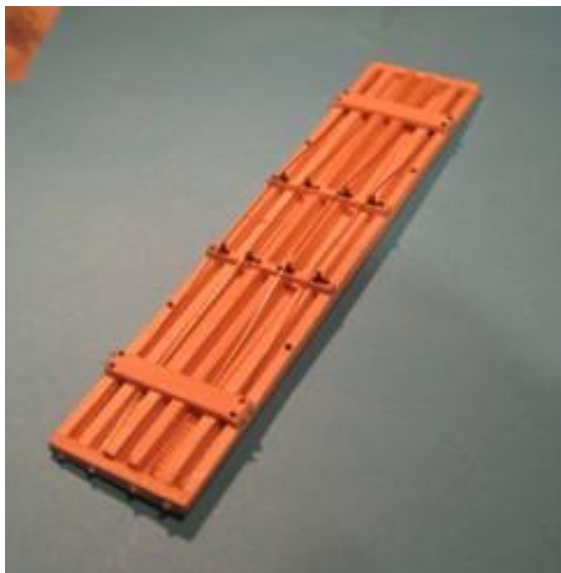


Fig. 11. Here is the final appearance of the floor/underframe with the longitudinal truss rod wires strung before going on to subsequent construction.



Fig. 12. Note the NBWs on the lower part of this boxcar's end siding that represent the connection point of the longitudinal truss rods.

Assembling the Basic Box

Gluing the Core Pieces Together. Now we will continue with constructing the basic box. Regardless of the type of floor chosen (framed vs solid wood), the ends are now glued to the floor. The photos from here on show a solid wood floor. If you are using a solid wood floor, be sure the pencil marks shown in Fig. 2 are facing out when gluing the ends. Put a thin wipe of glue on the top and bottom of an end block and place it on top of the floor board. Do the same to the second end block. Be sure to square them up as best as possible. Then place the roof board on the end blocks and tie off with rubber bands (Fig. 13). Note that the roof core wood from Northeastern has a wide overhang on the sides as they cannot know how thick of a scribed siding you will be using. This will be addressed below and is dependent upon whether your model will have a roof overhang.



Fig. 13. Assembling the basic box core by gluing the ends and roof to the floor.

Squaring Up the Box. It cannot be stressed enough that the next step is of great importance. This is where the core frame of the car kit is squared and flattened to accept all of the parts that follow; all the stuff that is visible when finished. Remove the rubber bands and examine the end blocks for location between the roof and floor boards. If an end block sticks out a bit, it must be sanded down to match the ends of the roof board.

When working on wood models, I do a thing called *decking down*. Decking down is a machining process when you need to flatten, or check for flat, of an item that will bolt to something. I put down a sheet of sandpaper on a flat surface and then work down the piece. So, for the end blocks, the trick here is to lay a sheet of 150 or 220 grit sandpaper on a very flat surface. A Formica-type bench top is perfect. Inspect your work after each stroke across the sandpaper (Fig. 14).

If the end blocks stick out a bit on a side they must be sanded down to the width of the roof and floor board. If there is to be no overhang on the roof

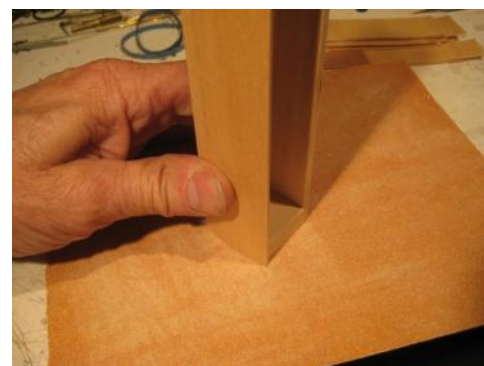


Fig. 14. Squaring up the top and bottom of the ends to the floor and roof.

board, simply lay the entire assembly flat on its side on the sandpaper and slide it across. If there is a roof overhang on the roof board, a slightly different approach must be taken. Place the sandpaper exactly at the edge of your work surface. The edge must be perfectly straight and square. Any rounding of the work edge will prevent this from working correctly. Then lay the car assembly on the sand paper with the overhang hooked over the edge of the work surface. This will prevent any material on the overhang from being removed. Do not fold the sandpaper over the edge of the work surface; only flush it up to the edge. This creates a perfectly straight and flat surface for the side panels when they are applied (Fig.15).

Inspect progress after every stroke. Basswood sands easily and quickly. Use the fingernail test to check for perfectly flat joints. If it is really flat, you will not feel a hitch across the joint in either direction (Fig. 16).

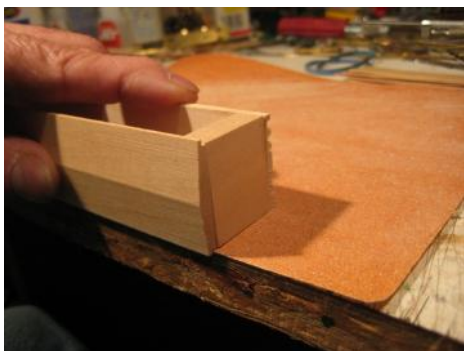


Fig. 15. Squaring up the end blocks if the roof will overhang the sides.



Fig. 16. The fingernail test to ensure edges are sanded even.

Roof Overhang. I use 1/32" thick scribed wood siding, so the Northeastern roof core sticks out way too far on the sides. Here is another decking down process that makes reducing the roof overhang really easy. Find a piece of sheet metal slightly thicker than what you want to have left as the roof overhang; that is, your siding thickness plus a few thousandths of an inch. Place the metal on a flat sheet of sandpaper and hook the roof overhang over the side edge of the metal. The roof core wood's overhang will be removed down to a point where nothing is touching the sandpaper. Can't miss—perfectly straight and even. I often use a steel ruler to do this.

Figure 17, shows me using a steel weight out of an old Athearn blue box box-car. If your spacer metal is not wide enough, then put the spacer metal near the edge of the sandpaper and let the lower edge of the car run out onto your smooth work surface.



Fig. 17. Sanding down excess roof overhang using an old Athearn steel weight as a guide.

Solid Wood Floor and Monofilament Truss Rods

Now for the most tedious part of a car build—tying the truss rods. If you did not use brass wire for the longitudinal truss rods as shown earlier, this works really well. I use .010" monofilament fishing line for HO models as it scales out at 3/4" diameter and it is flexible. If you try to use brass wire in this dimension and you even as much as touch it in any following step, it will bend, stretch, and NEVER go back to where it was. Fishing line is stretchy and will put up with handling. It takes paint reasonably well.

I use a single length of monofilament and thread it consecutively through all eight truss rod holes previously marked and drilled in the entire floor so that there are only two ends to knot and glue. The two ends of the string need to be on the inside of the car. Recall that Fig. 2 showed the various pencil lines for the truss rod holes in the floor. I do not put glue at each hole in the floor, only at the two knotted ends. This allows the string to equalize itself through the model when it is tensioned up.

Fishing line is notoriously slippery, so here are a few things to remember. NOTHING likes to stick to it and it is tough to get a knot to fall tight to the surface that you want it tight to. I learned this while fly fishing and modeling. Also, when pulling knots tight, spit on it a bit and the line will slide into a much tighter knot. My wife hates when I spit on stuff!!!!

Any time you want to secure it, tie a knot as close as possible to the hole where you want it. Lay it down on the floor of the car and then put a drop of CA (super glue) on it and in the hole. The glue really will not stick well to the fishing line but the knot cannot pull through the glue now filling the hole.

When you are setting the tension for the last time, first place the queen post castings flat on the floor under the four truss rod strings (Fig. 18). This is only the pre tension part of this. Do not attempt to actually load the strings up on the

queen posts yet as this puts too much stress on the floor board and puts an arch into it. The strings will not be fully tensioned until the scribed side panels are glued on to make the floor and roof rigid to each other. Knot off the second end of the string, taking out as much slack as possible but not pulling it very tight. It does not need to be an A string on a violin.

Pull the string tight on the inside of the car floor, then wedge a round toothpick into the hole from the underside (outside) of the floor to hold the string in place (Fig. 18). [I learned this from my dad when he took up re-caning chairs after his retirement from the Burlington RR. He had a ton of little pegs to hold things in place while he worked.] Tie a knot and slide it as close and tight as possible to the hole on the inside of the floor. Then put a drop of CA glue on the knot and into the hole. Let the CA set, then you can remove the toothpick.

For now, remove the queen post castings and let the lines go slack until much of the car is completed. When ready, reinsert the queen posts, stand them up and then move them a bit off of the pencil line and place a drop of CA. Slide the queen post onto the line and kick it off.

That's it for now. Part 2 will involve sheathing the ends, sides, and roof and adding fascia boards.



Fig. 18. Queen posts are laying flat while the monofilament string is tightened. Note the toothpick inserted into the last hole to hold the string tight while knotting and gluing the end.



Fig. 19. Truss rods and queen posts in final position.



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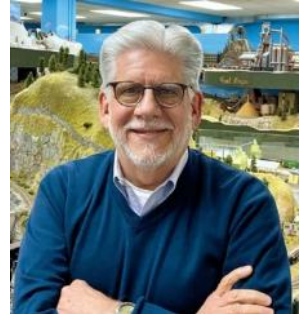
Officers' Reports

From the Engineer's Side of the Cab

Jeff Herrmann, MMR, PNR President

As you read this, I hope that you are packing for (or already on your way) to the 2025 PNR Convention "Ainsworth Connector." It is being held May 9-11 in Pasco, Washington. If not, it is probably not too late to decide to come! Our 5th Division hosts have changed a few things up this year and put together what promises to be a great convention...perhaps a 3-day event over a weekend will encourage more attendance?

Please take note of the articles elsewhere in this edition of *The Switchlist* or on the convention website <https://pnr5d.org/pnr-5th-division-home-page/2025-pnr-convention/> to see all the details of what is being offered. Whether it is first class clinics, layout tours, operating sessions, or our "A Celebration of Models, Photos, and Crafts" you will have opportunities to see what is great about our hobby and ask modelers about "how they did that!"



For many of us, catching up with old friends and making new ones is a highlight of the convention. So don't be afraid to grab a cup of coffee and join in on those casual conversations to talk trains while hanging out and relaxing. By the way, NMRA President Gordy Robinson and NMRA Western District Director Lee Calkins will be joining us at the convention and welcome the opportunity to chat with you.

I have mentioned to you before that PNR is taking action to make Modeling With The Masters® available to more NMRA members in our region and at a lower cost than what is typically available at national or regional conventions. My sincere thanks to Jack Hamilton, MMR, HLM, for spearheading this effort. On March 1st, we held our first event to certify MMRs as MWTM® instructors in Kennewick, Washington, and were able to "certify" ten individuals. We are currently planning similar certifying events for the Seattle/Tacoma area and western Canada to be conducted this fall. I am grateful for the MMRs who have stepped forward so far to join us in this effort. If you are an MMR and would like to help make MWTM available in your division, please let us know.

If you have some ideas you would like to share or think that I can be helpful in getting NMRA members more engaged or expanding our membership in your area, let me know. My focus, and that of our PNR Board of Directors, is to find ways to enhance the value of your NMRA membership.

I wish each one of you good health and clear tracks ahead! I am humbled and excited to serve as President of Pacific Northwest Region.

From the Fireman's Side of the Cab

Bob Stafford, Vice President

No report this quarter.



Treasurer's Report

Rick Martin, MMR

Greetings fellow Region members.

I trust that all are finding ways to adapt to Spring weather changes. Here in North Idaho our temps are trying to be Spring-like for the most part.

There is nothing significant to report at this time. The only thing as an update is a line item expense that I neglected to include previously. The item is for "Modeling With the Masters" sessions. A budget expense was approved by the board last

fall which was overlooked by me. I have since added it. Two sessions were planned with one already past several weeks ago. The second will be this fall likely in late September or early October.

The only other item I want to mention is regarding our funds. Many of you know that we have two Certificates of Deposit. One is for \$4,000 and the other is for \$10,000. Both are due for renewal toward the end of this year. Of course, this will require board approval to keep both, or not renew one and put those funds in our Money Market account.

The reason I bring up the CDs is that the Region does not generate income. Our annual budgets typically show a negative balance at the end of the year. I will include an updated budget spreadsheet for the next issue since it will be after the PNR Convention in May.

Having said the above, if any have questions, I would be more than happy to answer. Send me an email at rck.martin5727@gmail.com.



Office Manager's Report

This report to the members of the Pacific Northwest Region, NMRA, covers the months of April 2024 through March 2025. Because this edition of *The Switchlist* is being published a month earlier than usual, the membership numbers for April 2025 are not yet available. As you can see in Table 1, the Pacific Northwest Region, which consists of seven (7) divisions, currently has a total of 1,384 members, of which 10 are Rail Pass members. Over this same time period, unfortunately, we had a loss of 15 members due to death. These members are listed in the *In Memorium* section of the newsletter.

Table 2 shows that we brought in 117 new members over this same period, of which 25 are Rail Pass members. Please take any opportunity you can to welcome our new members.

Remember —Membership retention is the job of every member!!

Much of this report (Tables 1 through 4) centers on the relationship when individuals come on board as a new member and the time they go on the 30-60-day Past Due Reports. Look at the tables to see how your division is performing.

Table 1. Membership showing Rail Pass (RP) portion by Division

Div	Apr-24		May-24		Jun-24		Jul-24		Aug-24		Sep-24		Oct-24		Nov-24		Dec-24		Jan-25		Feb-25		Mar-25		Apr-25		Net +/- over year Apr-Mar
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	
1	76	1	77	2	76	1	81	2	81	1	79	2	75	2	76	2	72	2	76	2	75	2	72	1	NA		-4
2	109	2	108	2	106	0	109	0	111	0	106	0	104	0	108	0	110	0	108	0	110	1	112	1	NA		3
3	77	6	77	6	77	5	75	4	75	4	79	3	82	3	81	2	78	2	80	2	79	1	79	1	NA		2
4	440	8	443	7	446	4	449	4	448	6	448	5	447	4	434	3	433	4	432	4	429	4	432	4	NA		-8
5	203	2	202	2	205	2	198	2	200	3	197	2	198	3	197	4	197	4	196	3	195	3	198	3	NA		-5
6	257	1	256	1	257	1	252	1	266	1	241	1	242	0	245	0	243	0	240	0	241	0	241	0	NA		-16
7	279	6	279	4	280	6	260	5	273	3	275	1	269	2	267	0	265	0	250	0	261	0	250	0	NA		-29
Tot	1441	26	1442	24	1447	19	1424	18	1454	18	1425	14	1417	14	1408	11	1398	12	1382	11	1390	11	1384	10			-57

Table 2. New members showing Rail Pass portion by Division

Div	Apr-24		May-24		Jun-24		Jul-24		Aug-24		Sep-24		Oct-24		Nov-24		Dec-24		Jan-25		Feb-25		Mar-25		Apr-25		Total	
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP
1	0	0	0	1	2	0	1	1	0	0	0	0	0	0	1	0	0	0	1	1	0	0	1	0	NA		6	3
2	1	0	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	0	3	1	1	0	NA		7	1
3	1	2	0	0	1	0	0	0	2	1	1	0	1	1	0	0	0	0	1	0	1	0	1	1	NA		9	5
4	0	0	6	0	4	0	3	1	3	1	7	1	1	0	1	0	2	1	5	1	3	0	6	2	NA		41	7
5	3	1	0	0	0	0	0	0	2	0	1	0	1	0	1	1	3	1	1	0	1	0	5	2	NA		18	5
6	2	1	3	0	6	0	3	0	1	1	0	0	1	0	3	0	1	0	1	0	3	0	2	0	NA		26	2
7	2	1	1	0	1	1	0	0	0	0	0	0	1	0	3	0	0	0	0	0	1	0	1	0	NA		10	2
Tot	9	5	10	1	14	1	7	2	9	3	9	1	5	1	10	1	6	2	9	2	12	1	17	5			117	25

Table 3. Members 30-days Past Due showing Rail Pass portion by Division

Div	Apr-24		May-24		Jun-24		Jul-24		Aug-24		Sep-24		Oct-24		Nov-24		Dec-24		Jan-25		Feb-25		Mar-25		Apr-25	
	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP	Mem	RP
1	2	0	1	0	3	1	1	0	0	0	2	0	5	0	1	0	6	0	1	0	0	0	2	0	NSA	
2	2	0	1	0	0	1	0	0	0	0	3	0	4	0	1	0	0	0	2	0	1	0	1	0	NA	
3	0	0	0	0	0	1	1	1	2	1	1	1	0	0	1	0	3	0	1	0	2	0	3	0	NA	
4	3	0	7	1	2	1	5	1	7	0	9	0	4	0	14	0	3	0	10	0	8	0	12	0	NA	
5	2	1	2	0	0	0	7	0	0	0	2	0	2	0	4	0	4	1	5	0	3	0	3	0	NA	
6	1	0	7	0	4	0	8	0	0	0	11	0	1	0	0	0	4	0	4	0	2	0	5	0	NA	
7	3	0	2	0	1	0	1	0	7	1	0	0	7	0	5	2	2	0	14	0	5	0	5	0	NA	
Tot	13	1	20	1	10	4	23	2	16	2	28	1	23	0	26	2	22	1	37	0	21	0	31	0		

Table 4. Members 60-days Past Due showing Rail Pass portion by Division

Div	Apr-24 Mem RP		May-24 Mem RP		Jun-24 Mem RP		Jul-24 Mem RP		Aug-24 Mem RP		Sep-24 Mem RP		Oct-24 Mem RP		Nov-24 Mem RP		Dec-24 Mem RP		Jan-25 Mem RP		Feb-25 Mem RP		Mar-25 Mem RP		Apr-25 Mem RP	
1	4	1	2	0	1	0	3	1	1	0	0	0	0	0	4	0	1	0	5	0	0	0	0	0	NA	
2	3	0	2	0	1	0	1	1	0	0	0	0	3	0	2	0	0	0	0	0	2	0	1	0	NA	
3	2	0	0	0	0	0	1	1	2	1	1	1	1	1	0	0	1	0	2	0	1	0	1	0	NA	
4	7	0	2	0	4	1	2	0	5	1	6	0	8	0	3	0	13	0	3	0	8	0	5	0	NA	
5	2	1	4	1	2	0	0	0	8	0	0	0	1	0	2	0	3	0	4	1	5	0	2	0	NA	
6	12	0	1	0	6	0	4	0	8	0	0	0	9	0	1	0	0	0	2	0	4	0	2	0	NA	
7	0	0	2	0	3	0	1	0	1	0	7	1	0	0	7	0	5	2	2	0	2	0	4	0	NA	
Tot	30	2	13	1	17	1	12	3	25	2	14	2	22	1	19	0	23	2	18	1	22	0	15	0		

Achievement Program Report

Bill Fassett, MMR, AP Manager

Congratulate our newest Master Model Railroader®: Lee Chessman, MMR #789

On March 14, Lee Chessman was recognized as MMR #789. Lee is from the 5th Division, is a member of River City Modelers and the Inland Empire Free-mo group. He lives in Post Falls, Idaho. Lee's attainment of the MMR is somewhat unique in that it arises from modeling in 7-1/2" gauge and in HO. His Model Railroad Engineer Certificate – Civil and several of his structures were from his large-scale outdoor layout. Other structures and scenery awards were from the River City Western HO layout, and some structures and a Golden Spike Award were from his Free-mo modules. In addition to Model Railroad Engineer – Civil, Lee earned Master Builder – Cars, Master Builder – Scenery, Master Builder – Structures, Model Railroad Engineer – Electrical, Chief Dispatcher, and Association Volunteer.

Public Services Committee

As of April 1, there are three members who have continued to earn Volunteer time units by serving on the Public Services Committee. Two members are in Division 6 where they continue to work with a disabled individual to help him participate in the hobby, and the third is serving at the Puget Sound Model Railroad Engineers layout at the Washington State History Museum in Tacoma. This member has just completed his Association Volunteer certificate using, in part, points from service on the PSC.

AP Evaluations at the PNR Convention

As we have done for the last two conventions, evaluations of models for Merit Awards will be by appointments where the modeler, and any attendees interested in observing the process, will meet with the evaluators in the Celebration Room. Appointments will be available on Friday and Saturday, and a sign-up sheet will be posted at the Celebration Room check-in desk. For more information, check out the convention [webpage](#).

Awards Earned Since the Last *Switchlist*. [division numbers in brackets]

Golden Spike Award – The following members have been awarded a Golden Spike certificate:

James Durst [4]
 Daniel Haigh [4]
 Jim Landers [4]
 Ryan Matthews [4]
 Robert Murstig [5]
 Dennis Terpstra [4]

Merit Awards – The following members have earned Merit Awards:

Michael Andersen [5] – Structures
 Alan Armbruster [4] – Scenery
 Cory Bennett [5] – Structures
 Chris Carson [2] – Structures (6)
 Lee Chessman [4] – Cars
 James Durst [4] – Prototype Models; Scenery; Structures (2)
 Trevor Dring [6] – Civil (3)
 Lawrence Ellison [4] – Scenery
 Daniel Haigh [4] – Scenery

Doug Hicks [7] – Civil (3)
Russell Kerr [4] – Motive Power (2)
James Keena [3] – Cars
Jim Landers [4] – Scenery, Structures (6)
John McDonald [5] – Structures
Stu Sibitzky [LSR 1] – Structures
Dennis Terpstra [4] – Scenery

Achievement Program Certificates – The following member have earned AP Certificates:

Alan Armbruster [4]—Model Railroad Engineer – Electrical; Master Builder – Scenery
Frank Ascolese [5]—Master Builder – Scenery
Chris Carson [2]—Master Builder – Structures
Lee Chessman [5]—Master Builder – Cars; Chief Dispatcher; Master Model Railroader®
James Durst [4]—Chief Dispatcher; Model Railroad Engineer – Electrical; Master Builder – Scenery; Master Builder – Prototype Models
Lawrence Ellison [4]—Model Railroad Engineer – Electrical; Master Builder – Scenery
James Farnham [4]—Chief Dispatcher
Daniel Haigh [4]—Model Railroad Engineer – Electrical; Master Builder – Scenery
Gordon Ingraham [5]—Association Volunteer
Greg Kujawa [5]—Master Builder – Prototype Models
Keith Nore [4]—Model Railroad Author
Izzak Shaw [4]—Chief Dispatcher
Dennis Terpstra [4]—Model Railroad Engineer – Electrical; Master Builder – Scenery

THE PNR AP TEAM CONGRATULATES THESE MODELERS

Selected Pictures – North to Alaska

Greg Wright, MMR, and I journeyed to Fairbanks, Alaska, in March and visited with the members of the 100% NMRA club, the Tanana Valley Model Railroad Club. As a result of the visit, 21 AP certificates were issued, many of them reflected in this report.

Here are two photos from their layout, which is located in the Alaska Railroad Fairbanks Depot.. The panorama is of the Tanana Valley portion of the layout, and the peninsula is the Mt. Healy portion. The layout is open to the public every morning from May through September.



Division Reports

1st Division

Charlie Hutto, Superintendent

First Division is hoping to hold a mini meet later this spring, as well as another in the fall. We will get additional details to our members when details are worked out. Otherwise, we encourage all members to attend the Regional Convention in May.

2nd Division

Jeff Shultz, Superintendent

2nd Division took the month of March off due to Winterail and the Willamette Model Railroad Club swap meet, but a short meet at the Wilsonville Library followed by dinner at the Black Bear Diner was held on April 13th. Twelve people showed up for the meet, where Superintendent Jeff Shultz provided a presentation on his home layout, Phil Selinger had a presentation on the Ma & Pa RR, and Bill Decker showed off a couple of his standard SP depots. Eleven people continued on to dinner at the diner, where many active conversations ensued.

The next meet will be at the Wilsonville Library on Saturday May 17th from 1 pm to 5 pm.

3rd Division

Shelley Shelstad, Superintendent

Ah springtime! The 3rd Division just held its annual Spring Meet. Featured events of the full-day event included a fantastic weathering clinic by James Keena, AP judging, popular model contest, silent auction, division business meeting, and three layouts to tour. Thirty-three members attended (over 40% of our active membership).

4th Division

Mike Stepner, Superintendent

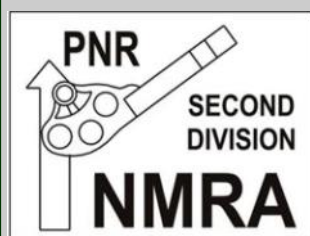
4D Free-mo is setting up at Puyallup fairgrounds for Spring Fair. This is the third year, I believe.

Cole Van Gerpen of CVG Trains is 4D's new education chairperson.

Magnus and Robin have worked diligently to get 4D to host the NMRA 2027 national convention in Tacoma. If anyone would like to help out, we could use it. Contact a BOD member.

We have three open BOD positions and five folks running. Please keep an eye out for ballots and vote.

The PNR's 2025 convention, [Ainsworth Connector](#), is being hosted by the 5th Division in Pasco, and is quickly approaching. If you can, please attend.



2nd Division Meets Online

2nd Division has scheduled Meets on the 3rd Saturday of each month. For more info, see: <https://2dpnr.org/html/events.html>. Recorded Zoom Meets are at: <https://www.youtube.com/channel/UCWMrJ-wg0pHqoke3px4F66g>

5th Division

Bill Voogd, Superintendent

Really nothing much to report from 5th Division other than we're totally focused on convention prep at this point. Our annual meeting is April 19. Upcoming train shows are in Livingston and Helena on April 26 and 27, respectively.

That's all for now. I look forward to seeing many of you at the convention in Pasco.

6th Division

Ed Molenkamp, Superintendent

No report this quarter..

7th Division

Victor Gilbert, Superintendent

The 2025 Train Show Season has started with a new show that we hope will continue to be an annual event.

On March 1st, the Yarrow Community Model Train Swap Meet had many enthusiastic attendees and was very welcome to the modellers of the eastern Fraser Valley.

The Sea to Sky Model Train and Hobby Show came next, and the Nanaimo Model Railroad Show just rapped on April 13th. Our flagship meet, the [Railway Modellers Meet of British Columbia](#) (RMMBC), will be held from May 23rd through the 25th at Simon Fraser University in Burnaby, BC.

Like other organizations, the Lower Mainland Train Expo Society (LMTES), the legal entity that underlies the Vancouver Train Expo (VTEEx) every year, is having trouble finding volunteers. To this end, it is likely that the VTEEx show will not happen in 2025. Whether it returns in the future is also up in the air.

The Division continues to embrace membership challenges and is working to retain its current members.

As always, I want to thank the 7th Division members who continue to deliver an exceptional experience to the Model Railroad community, virtual or in-person.



4th Division Online Resources Open to All

The 4th Division has a lot of resources available on its *Grab Iron* webpage: <http://4dpnr.com/grab-iron-posts/>

These include announcements of upcoming online clinics and layout tours plus a YouTube Channel that includes recordings of past clinics and layout tours and other presentations. Here is the link: <https://www.youtube.com/c/4DPNRMovies>

PNR Interchange

The **PNR Interchange** is a free service for PNR members wishing to buy, sell, donate, or exchange model or prototype railroad related items. Commercial enterprise transactions and advertisements are not allowed except as provided for hobby stores below. Members may also use this service to request or solicit hobby or railroad related information, research results, and other resources or materials from other members. Members' classified ads and notices are restricted to text format only (no graphics) and length should be no longer than 120 words. Sequential ads of the same nature or having the same or nearly same content are not permitted. The suitability of submitted classified ads for publication is determined at the sole discretion of the Switchlist Editor. Email your submission along with your NMRA membership number to the Editor at switchlist@pnr-nmra.org. Hobby stores that support model railroading may include an advertisement in PNR Switchlist. The ad is limited in size to a business card size and should be provided in ready to copy and print format as determined by the Switchlist Editor. Advertisements for hobby stores that provide a discount for NMRA members shall be so identified with the advertisement. Hobby store ads may be run sequentially issue to issue.

FOR SALE: 2001 PNR Okanagan Convention cars. (HO Scale-National Steel 40' Box Car w/8' door, NSC-3 ends, NSC roof & roofwalk). One car for each of the CN & CP Railways by Trains Canada. Never run. \$15 EACH or \$25 for the pair. Shipping NOT included. For further info or pics, email: mdadams006@aol.com

05/25

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Searching for that "special run" or hard-to-find locomotive?
Looking for prototype or historical photos, track diagrams, or other information?
Place your classified ad here in the PNR Interchange and connect with fellow PNR members!

We were asked by MCR Cincinnati Division 7 to make the PNR membership aware of the following opportunity. –Editor

Limited-Edition, HO Scale N&W HC-46 ACF 2-Bay Covered Hopper Available for Sale/Purchase Through MCR, Cincinnati Division 7

Cincinnati Division 7 is offering a limited-edition, HO scale freight car for sale/purchase: a N&W HC-46 ACF 2-Bay Covered Hopper in a unique repaint scheme with patched-out data.

The following link provides details about the car as well as pricing, shipping, and ordering information:
<https://division7mid-centralregionnmra.godaddysites.com/car-projects>

We thank you in advance for your support!

Paul Maciulewicz

NMRA; MCR; Cincinnati Division 7
Car Projects Chairman



The *NMRA Turntable* makes the Internet a little smaller.

If you're spending hours doing Internet video searches, you're wasting valuable modeling time! The *NMRA Turntable* brings the best of the best model railroading videos to your email box every month. It's one more benefit of NMRA membership!

If you're not receiving it, contact NMRA HQ and make sure they have your current email address.

Timetable

To add your events or activities to the Switchlist Timetable, PNR web page, and PNR Train Orders, contact the Timetable Editor, Peter Ulvestad at timetable@pnr.nmra.org.

Also, if you want to add your events or activities to the NMRA.org Calendar, please send the event information via email to calendar@nmra.org

2025

April 19, Edmonton, AB

MMRF Spring Swap Meet - 2025 Central Lions Seniors Recreation Centre, 11113 - 113 Street, Edmonton, Alberta 11 AM - 3 PM. Free parking in rear. More information available at https://www.mmrf.ab.ca/spring_swap_meet

April 26-27, Regina, SK

Regina Railfest 2025 – Model Railroad Show. Over 25,000 sq. ft. of model railroad layouts, displays, exhibits, and vendors. Caledonian Curling Club, 2225 Sandra Schmirler Way. More information available at www.reginarailfest.com

April 26, Livingston, MT

33rd Annual Livingston Swap Meet. Livingston Depot Center, 200 W. Park St, Livingston, MT 59047. 9:00 am–2:00 pm.

April 27, Helena, MT

[43rd Annual Helena Railroad Fair](http://43rdAnnualHelenaRailroadFair.com). Helena Civic Center, 340 Neill Ave, Helena, MT 59601. 9:30 a.m. – 4:00 p.m. Admission \$5; under 12 free. The largest railroad hobby event in the Northern Rockies. Up to 200 tables of railroad items, tinplate and toy trains, scale models, railroad collectibles. Buy, sell, or swap. Info: 406-459-3470; email: rrfair@mt.net.

May 9-11, Pasco, WA

2025 PNR Convention, "[The Ainsworth Connector](http://TheAinsworthConnector.com)". Pasco Red Lion Hotel and Conference Center. Activities include self-guided layout tours; Free-mo layouts; operating sessions; clinics; Celebration Room; AP evaluations; non-rail program; Annual General Meeting; Sunday awards luncheon with keynote speaker, raffles and prizes; Modeling With the Masters (pre-convention clinic on May 8). Info: <https://pnr5d.org/pnr-5th-division-home-page/2025-pnr-convention/>



May 9-10, Calgary, AB

Railroad Prototype Modellers Meet. Theme: Freight Car Loads. St Andrews Presbyterian Church, 703 Heritage Drive SW, Calgary, AB. Six clinics and associated photo night. Cost is \$15. Bring your load lumber loads, coal loads, wide loads. There is no limit to the number of items you bring to show. Co-sponsored by the 6th Div of the NMRA. For further information and updates, contact Dave Audley, 403-275-1869 (daudley@telusplanet.net) or Dale Sproule (sproules@telusplanet.net).

May 23-25, Burnaby, BC

[Railway Modellers' Meet of British Columbia](http://RailwayModellersMeetofBritishColumbia.com)

June 6, North Cooking Lake, AB

The Mainline Model Railroaders Fellowship (MMRF) annual Trainspotting BBQ, Friday June 6 16:00-22:00. Deville - North Cooking Lake Community Hall, 21033 Wye Road (that's 20 km east of the Hwy 21 & Wye Road intersection), with the CN mainline across the road. If the weather is poor (let's hope not), there's a hall for shelter. Burgers, hotdogs and PG refreshments are provided free of cost so all you need to bring is your lawn chair and camera. Your significant other and children are also welcome to attend.

July 14-19, Novi, MI

2025 NMRA National Convention [Station No.VI](http://StationNo.VI.com). Hosted by the North Central Region of the NMRA.

September 13-14, Edmonton, AB

Greater Edmonton Model Train Show. **New location:** To be held at the Edmonton EXPO Centre. Activities for the whole family including the LEGO Activity Centre, operating layouts, How-to clinics & demonstrations, static displays, and dealer booths. More information to come: <https://www.mmrf.ab.ca/>.

October 18-19, Melville, SK

Melville Model Train Show. CN Community Centre, 10:00 a.m. till 4:30 p.m. both days. Cost: \$10.00 - adults, \$5.00 - youth 6-17, \$25.00 - family, free - under 6. For more info, email Ron at melvillemodelrailwayclub@outlook.com

October 25, Edmonton, AB

MMRF Fall Swap Meet. Central Lions Seniors Recreation Centre, 11113 - 113 Street, Edmonton, Alberta. 11 a.m. – 3 p.m. Free parking in rear. More information to come at <https://www.mmrf.ab.ca>

NOTE: Because event conditions may change rapidly, be sure to double-check the status of specific events by visiting the PNR website's list of Upcoming Events, the website of individual events, or contact the person in the specific event's listing.

NATIONAL MODEL RAILROAD ASSOCIATION
2025 National Convention

STATION No. VI

Hosted by:
the NORTH CENTRAL REGION

July 14-19, 2025

Open to ALL model railroaders! You do not have to be an NMRA member to attend! **REGISTRATION IS OPEN!!**

Our location – Sheraton Detroit-Novi, MI
A suburban location with FREE parking!

CONVENTION GOALS and HIGHLIGHTS -

- ** Lowest possible entry costs for individual, spouse and children
- ** Everyone pays the same prices!!
- ** Shorter convention starting 1pm Monday and continuing through Saturday afternoon with a banquet Saturday evening.
- ** Registration includes all layout visits, operating sessions, clinics and admittance to the National Train Show. (*Modeling with the Masters and some proto-tours are extra costs*)
- ** Support for all the traditional activities of the SIG's.
- ** National Contest will be fully supported and run by the National Contest team.
- ** Model Display room for those simply wishing to display models. This also includes the option of entering the People's Choice Contest.
- ** We will have several prototype tours
- ** We expect to have at least 90 clinics and each will be given twice.
- ** We expect to have at least 60 layouts open for tours.
- ** We expect to have up to 20 layouts with OP sessions. All layout tours and op sessions will be self-guided so you can take your time.
- ** We will have a YARD SALE room. Bring stuff to sell! (*NOT an acution!*)
- ** Pizza Party on Friday night!
- ** Two Day National Train Show, Saturday & Sunday

<https://nmra2025.com/>



NMRA Calendar Online

Are you interested in knowing what NMRA-sponsored events, other model railroading events, and train shows are scheduled beyond the PNR boundaries?

Then check out the online **NMRA Calendar**.

The calendar is available at: <https://www.nmra.org/calendar>

If you want your upcoming event posted on the NMRA calendar, send the event information to calendar@nmra.org.

Pacific Northwest Region Staff

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Pacific Northwest Region National Model Railroad Association

-  **Division 1:** Southwestern Oregon
-  **Division 2:** Northwestern Oregon, Southwestern Washington
-  **Division 3:** Southern Idaho, Eastern Oregon
-  **Division 4:** Western Washington, Alaska
-  **Division 5:** Eastern Washington, Northeastern Oregon, Northern Idaho, Montana
-  **Division 6:** Alberta, Saskatchewan, Northwest Territories
-  **Division 7:** British Columbia, Yukon

