

Highball!

OFFICIAL PUBLICATION 6TH DIVISION PNR

December 2024



Doug Airey's Mogul River Railroad was one of the 10 layouts open for operating sessions at **Northern Rails** in Edmonton, Calgary, and the Camrose area in late September. Doug's layout features great Northern Ontario scenery with photo backdrops and nicely detailed trains. (photo by Rob Badmington)

See the 6th Division Officer Reports, 9 Area Reports, and much more in this issue!

Officers of the 6th Division

Elected Officers

Superintendent: Ed Molenkamp
Phone: 780-886-8116
Email: 6divsuper@pnr.nmra.org

Assistant Superintendent: Rick Astle
Phone: 403-507-3314
Email: 6divasstsuper@pnr.nmra.org

Appointed Officers

Secretary: Doug Johnson
Email: 6divsecretary@pnr.nmra.org

Treasurer: Rick Walker
Email: 6divtreasurer@pnr.nmra.org

A.P. Chair: Doug Burton, MMR
Email: 6div-apchair@pnr.nmra.org

A.P. Vice-Chair: Dennis Dreher, MMR
Email: 6div-apvicechair@pnr.nmra.org

Highball! Editor: Rob Badmington
Email: highball@pnr.nmra.org

Convention Coordinator:
volunteer today!

Layout Design / Operations: Cal Sexsmith
Email: 6div-opsld@pnr.nmra.org

Webmaster: Peter Ulvestad
Email: webmaster@pnr.nmra.org

Superintendent's Report – Ed Molenkamp, Supt. 6th Division, President NMRA Canada

Greetings! The year has flown by and now we are on the last issue of Highball! in 2024 already.

The year 2025 will bring with it many new things including Shows. The Division should be at each show this year including: Prince Albert, Saskatoon, Moose Jaw, SUPERTRAIN, and Regina.

Speaking of upcoming events, the Convention planning is well under way. The plan at this time is to hold the Convention in Hinton, Alberta. We will be finalizing a date by the end of the year. We are in the process of

negotiating with a couple of hotels but as they are still dealing with folks from the Jasper fire, the availability of rooms is still up in the air. We are hoping to have the date set for a weekend in June like last year but as you can see there are a lot of events in May/June. This is however a good problem to have these days because it shows there is still enthusiasm for the hobby.

I hope you all have a Merry Christmas and a Happy New Year!

Ed.

Assistant Superintendent's Report – Rick Astle

Time flies when you are having fun and Christmas is just about upon us. I hope all is well with members and their families this time of year.

I have been busy with events in Didsbury and Olds this fall and the highlight of my season so far is receiving my nicely crafted Empress locomotive from Rapido. I have been running it on my home layout and it looks right at home.

Do you have a special project for the winter months ahead? If so how about sharing your progress with a short article and some photos. Your efforts may just be what another member needs to get a project moving forward. I asked what I have done to promote NMRA recently so I will ask it again here.

How can I help NMRA and 6th Division PNR in particular? Since I ask this question of myself I think it only fitting to ask it of you as well and here is one option we can all work on. Do I know someone who likes model trains as I do but needs help with the hobby or is there a former member who has not renewed that still has a passion for trains? How about offering to mentor them with their layout or just bring them to a club meeting or operating session and let them runs some trains. You won't get them to join without showing them what NMRA has done for you and can do for them in their journey. Remember we are NMRA and we can make it better by being there to help even one budding model railroader.

See you at the next stop, Rick.

From the Editor – Rob Badmington

Once again I am overwhelmed by the quantity and quality of material that has been contributed by members for your reading pleasure in Highball! Thank you to all who sent stuff. More is always welcome from anyone who would like to contribute.

There are lots of events coming up in 2025 in all corners of the Division. I hope to see you at some of these.

As you can see at the top of page 2, 6th Division Officers now have new email addresses so personal addresses don't have to be used. Peter has also revamped the division's website. It's a great place to get all the up-to-date info on what is happening.

Wishing you all the best for a Merry Christmas and Happy New Year!

Achievement Program Report - Doug Burton MMR, 6th Division, PNR AP Chair

This fall has been a busy time for AP. I have had 3 people volunteer to help out in Calgary and Regina. Their title will be District Associates: Al Matchett and Dave Audley in Calgary and Malcolm Anderson Regina. They will help arrange assessments and be the local contact if you need something. Their contact information will be on the web page.

I have also co-sponsored the Prototype Modellers Meets in Edmonton and Calgary. We will be looking for people to put on clinics and provide show and tell models to make these events successful. Participants in these events do not have to be NMRA members. They just need people who appreciate great modelling. What this means is that anyone who does clinics at these meets can get credit for their work. This can be used for either Author or Volunteer. I will also co-sponsor the layout tours in Edmonton.

We are finding that as members take a look at the AP eligibility information they have more completed than they

realize. Usually this involves scenery, electrical and civil. If the paper work scares you please talk to us as in most cases the paperwork can be reduced.

Once again Northern Rails was very successful. A big shout out to the organizing committee. This will also be an event that we will co-sponsor in the future.

In my next report I will publish the numbers for 2024 6th Div AP recipients and there are a lot of you. Congratulations to all.

The farm toy crowd is slowly moving towards building modules to display their equipment. If you know someone who is a collector, reach out to them to attend some of our prototype events. They are currently doing more and more scratch building of models, as such it is an opportunity for all of us to learn something new.

I wish you all a Merry Christmas and a Happy New Year.

Area Reports

Camrose – David Halliday

Battle River Railway Modellers had an ambitious plan for the club's return to the 2024 Greater Edmonton Train Show. That plan centred on building a new double-track layout that would be lighter and more compact to make it easier to move and set up.

Months of work went right down to the wire in building and landscaping the modules of the new layout. At 3 p.m. on Sept 13, we loaded the modules into the trailer for the trip to Millennium Place in Sherwood Park. Once there, we went to work setting up the layout and completing the wiring.

For this layout, we used thinner plywood, lightened further by drilling large holes with a hole saw in the inner members — similar to the method used to lighten cars for racing. While we used 3/4 inch plywood for the roadbed, the rest of the top of the modules used styrofoam as a base for buildings and scenery. Brett McDonald's design for the curves on the corners was much more compact, allowing for one module instead of the two used previously.



Much of the track was flex track, but Norm Prestage hand-laid the track on his module, including the switches. Jonas Creighton's module with a flying saucer and a wheat field attracted interest from spectators. The scene includes X Files agents Mulder and Scully investigating the alien landing.

But the adventure didn't end with getting to the show. As Ron Hillis drove home later that night his trailer was T-boned at an intersection less than a mile from his house. The trailer was a write-off and so was the vehicle that failed to stop and hit the trailer.

Fortunately, Ron had another trailer that we used to transport the layout after the show.



We plan to do more work to the layout, but we're ready to return to GETS next year.

Olds - Didsbury - Carstairs - Rick Astle

The Mountain View Model Railroad Club has enjoyed a great summer and fall showcasing our indoor HO layout as well as our outdoor garden railroad for our many visitors to the Didsbury Museum.

This year we focused on improving some of our scenery and just having fun running trains.

We have taken an active role with Didsbury's Christmas event on December 13th and set up at the train station to promote our club and welcome CPKC Holiday Train.



We look forward to the winter as the many shows and layout activities move indoors. We hope to encourage new memberships by sponsoring some fun operating sessions for existing members as well as those with trains of their own in either DC or DCC. Our 2024-25 meeting schedule is on our web site at

www.mvmrc.ca/page2.html.

Moose Jaw - Thunder Creek Model Railway Club – Al Howard

The Thunder Creek Model Railway Club continues to operate every Friday & Saturday at the Town and Country Mall at 1235 Main Street North in Moose Jaw. We have an operating HO display plus a static N Scale display as

well as several other items of interest. Operating sessions are open to the public at no charge.

We had a very successful Halloween day at the mall seeing about 850 kids go through our display area. We

are looking forward to a busy Christmas season at the mall as well. The club has also set up our annual Christmas train display at the Western Development Museum.



Our annual Christmas Train display at the Western Development Museum

Five members of the club attended the Melville Model Train show, Oct 19-20. We were pleased with the show and we thank the Melville club for their hospitality and for putting on a well-organized show.



Some fans enjoying our display at the Melville show

Our Annual General Meeting was held on Oct 29. Despite experiencing a modest loss during the past year, the club voted to keep membership dues the same as the previous year. Barry Johnson, Gary Trumbley and Gilles Doiron were returned as President, Vice President and Treasurer respectively.

We are presently planning for our Annual Show, to be held March 22-23, 2025, at the Western Development Museum in Moose Jaw. Notification and invitations will be sent out early in the New Year.

Regina – Echo Valley Railroad Guild – Doug Johnson

The club's Free-Mo group was at the Winnipeg show in September and reported that it was a well-attended and successful event. The Free-Mo group is planning on attending 2025 events in Prince Albert in January, Saskatoon in February, and Moose Jaw in March. The G scale group and the On30 group will likely be at the Moose Jaw show. The new On30 layout will be on display there for the first time. All of the groups will be

at the 2025 Regina Railfest show which will be at the Caledonian Curling Club facility on April 26 and 27. Planning is underway for that show. In the meantime, members have been working on home and modular layouts. Malcolm Anderson has been busy building rolling stock. Pictures and descriptions are included in this Highball!.

Airdrie / Iron Horse Park – Greg Orme

Hello everyone. I have a lot to share with you since my last report in August so let's get started.

Unfortunately, it starts off on a sad note. On September 17 we had to say a final goodbye to a good friend and fellow model railroader Alan Pile. Alan had been a member of Iron Horse Park since 2005. He had held the position of president and vice president and had just

become a life member last March. Alan was predeceased by his wife Patricia who passed away in 2020. Both Alan and Patricia had been very active members with the Alberta Model Engineering Society and will be sorely missed.

Some of our Edmonton members and a couple of Calgary members set up a static display at the Greater Edmonton

Train Show held Saturday and Sunday the 14th and 15th of September in the Millennium Place in Sherwood Park. From what I hear it was a great success and we have been asked if we could set up rides at next year's show.

Our AMES season wrap up play day and barbeque was held on Friday, 27 September, a beautiful late summer day. Over 40 members attended and had a very enjoyable time. Many thanks to the volunteers' efforts to put it on.

One of our junior members was assigned custodianship of two GE 20 ton box cab locomotives that were acquired in 2013. They had been sitting at the back of our local shed since then. Jaymes Gagne worked diligently to get these two box cabs up and running before the end of season.

There are the many ways AMES supports the local community and one of the ways is to share the park with other clubs and non-profits. One of these clubs is the Time Travellers Car Club of Airdrie that was founded in 2006 by a group of local hot rod and muscle car enthusiasts. They have been holding their monthly meetings in the station at Iron Horse Park and on Friday, September 13 they presented AMES with a plaque in appreciation.

Our regular run days ended on the Thanksgiving long weekend, Sunday, October 13, with a grand total of 14,825 journeys provided through the season.

One of the other ways we support the community is our yearly Halloween run. This event was held on Sunday, October 26 and had 812 people turn up in support of the Airdrie Food Bank. Fortunately the weather was very good for late October and we were able to collect donations of 680 pounds of non-perishable food with the equivalent cash value of \$2434. Along with the food donations, through journey tickets and cash donations we were able to add \$913 to that amount for a total donation of \$3347. The additional 812 guests brought our grand total to 15,637.

Through the generosity of the Alberta Airdrie Registries' office we were the recipients of a replica scale model of

the iconic Airdrie water tower. It is now sitting in front of our station and has made a great addition to our miniature buildings.

On Friday, November 15 after a three month stay at the Rockyview Hospital, another one of our members, Brian Tonner passed away. Brian was instrumental in the rebuilding of our CN 6218, 4-8-4 Northern steam locomotive.

This year's recipient of the Jim Swan Award was Airdrie Cooper Crossing School. Jim Swan who passed away in 2021, was a member of AMES and one of his concerns was that young children often went to school hungry. In his honour, the annual Jim Swan Award was initiated where AMES donates \$1000 to a local school meals program.

And yet more sad news as our third member in as many months has passed away. Longtime AMES member and fellow model railroader Ian Tucker passed away at the Chinook Care Centre hospice in Calgary on December 2 after a courageous battle with cancer. Our condolences go out to his wife, Sandie and family. A celebration of Ian's life is scheduled for Saturday, 14 December at the McGinnis Holloway funeral home.

As stated in previous reports there is never an end to the work required to keep things interesting and the trains running throughout the year at Iron Horse Park so if you find yourself with some extra time on your hands, we are always looking for new members or volunteers to help out. If you would like to get involved, we would be happy if you would consider joining us.

Send me an e-mail or check out the Information that is available on our Web site.

This being the last report of the year I would like to wish everyone a very Merry Christmas and a Happy and Healthy New Year.

Till next time Happy Railroading....

Saskatoon – Roger deWeerd

Prairie Rail Workshop has had an active October hosting a 2nd open house on the 20th and our swap meet on the 27th Our next swap meet is March 2, 2025. If you would like to participate in the swap meet, please email prswapmeet@gmail.com. We will look into hosting another open house at our clubhouse in Saskatoon, possibly during the Christmas break. Until then stop by and visit with us and the crew from Rocky Ridge Model Railway Club at Saskatoon's Lawson Heights Mall where

Rocky Ridge has their wonderful HO scale layout set up and Prairie Rail has their T Track layout set up. They are located close to Santa, so when you come to tell Santa your model railroad Christmas list, stop in, say hello and enjoy the layout displays.

Speaking of displays, mark your calendars as the All Aboard Model Train Show is fast approaching, Feb 15 and 16, 2025. Come celebrate our 40th Train Show in

Saskatoon, and we are planning the largest show we have ever hosted. We are working on having many layouts from Western Canada, vendors and some manufacturers represented at our show. If you would like to participate as a vendor, manufacturer or bring a layout to the show, please reach out to us at allaboardtrainshow@gmail.com. Follow us on our

Facebook page for both the Prairie Rail Workshop and All Aboard Train Show for more information on our events, open houses, and general information.

Have a Merry Christmas and Happy New Year, may you find lots of model train items, not underwear under the tree from Santa..

Edmonton – Peter Ulvestad

The MMRF hosted GETS on September 14/15 followed by the Swap Meet on October 19th. Planning is already underway for GETS next year as well as the Spring Swap Meet. <https://www.mmrf.ab.ca/>

The biannual Northern Rails was held in September. It featured 8 layouts in Edmonton and the Camrose area, plus a bonus layout in Calgary. In addition, the event

included a streetcar ride over the High Level Bridge and a BBQ at the Fort Edmonton Freight shed.

The Edmonton Model Railroad Association hosted an operating session for Northern Rails during the operations weekend. The EMRA held their annual Christmas party December 10 and will be winding down for the Christmas break, reconvening January 7. Planning continues on the layout expansion.

Lethbridge – Paul Smith

The Southern Alberta Model Railway Club had a busy fall. We experienced a good turnout from the public for our Fall Open House on October 26th and 27th. Doug Wingfield provided us with a walk down memory lane courtesy of a photo night featuring railways in South Africa and southern Ontario on November 19th. Our committees are moving forward on operating procedures and structure/scenery construction for our HO scale layout and a new layout plan for our N scale layout.

We're organizing matters for the coming year. We plan to hold a silent auction of items that have been accumulating around our clubhouse; our tentative schedule will be from January 14th to February 4th.

We've scheduled our Annual Social for February 8th and our Semi-Annual General Meeting for February 25th. Our Spring Open House is coming up in March. These events are open to visitors, except for our AGM (please note that you'll have to make arrangements if you participate in our silent auction, and that there is a fee for the Social). Check out our web site for further details.

The SAMRC meets Tuesday evenings at our Gyro Park clubhouse. As always, any Highball reader who finds themselves in Lethbridge at that time is welcome to visit with us. We'll close with wishing everyone the best of the season and for the coming year.

Calgary – Dale Sproule

CMRS has been busy hosting their annual Calgary events: The fall CMRS mini meet had about 70 members attend the mini-meet and AGM. Upcoming events: the annual CMRS Slide Night, Layout Tours (details for CMRS members will be forth coming), and the annual CMRS Spring Mini Meet. The Calgary Model Trainmens' Club is hosting their annual Flea Market in conjunction with the Spring Mini Meet at the Glenmore Inn, March 2, 2025, 9:00am - 3:00pm. SUPERTRAIN 2025 will be held in the Olympic Oval at the University of Calgary April 5-6, 2025. This venue has more than twice the floor area of the Genesis Centre, with plenty of parking nearby.

It was with sadness I read that Laurie Green from Australia has passed away. I met him in Boston, a wonderful narrow gauge modeller. Closer to home, Eric Courtney, founder of "Minatures by Eric" also passed away recently.

A review of the 2024 PNR meet held in Surrey, B.C. in the latest PNR Switchlist has some 209 attendees. Roger Walker from the 6th Division was the big winner in the contest area taking 2nd place in freight cars with his D&RGW Reefer #45, 1st place MOW for his non-revenue, D&RGW On3 Flanger OD, 1st place in structures for his Water Tower, 1st place for his Photography-Prototype 2-6-0 #1057, and not surprisingly his People's Choice-Best

Model Roger Walker Sawmill. Out of the 209 or so folks there were 19 Albertans. I do not recognize any other 6th Division member taking 1st or 2nd place in any category, perhaps 3rd? Hopefully at the next PNR meet this will change! There is my challenge 6th Division members who will attend the next PNR meet in Pasco, Washington, . . . take something, anything. The organizers will appreciate it. The Celebration of Models, Photos, and Crafts (Contest Room) was well received with a variety of trains, structures, photos, crafts, and displays set out for viewing. A total of 84 items were logged into the room. Thank you to all who brought a project.

Calgary Model Trainmen and Tanana Valley Model Railroad Club also received special recognition from the NMRA at Surrey. Jeff Herrmann MMR, PNR President awarded The Calgary Model Trainmen Club the PNR Empire Builder Award for 75 years of continuous operation; and the Tanana Valley Model Railroad Club in Fairbanks, Alaska, received the PNR Mountaineer Award for 40 years of continuous operation.

Pasco, Washington is the next location for the PNR so plan to visit the Tri-Cities in 2025, May 9-11, 2025. Registration and Hotel accommodation is already open. See <https://pnr5d.org/pnr-5th-division>

CMT (address: #7-11 Street N.E., Calgary - North of the LRT and West of the Zoo and Tom Campbell Hill) is having Free-Mo setup their modules next to the CMT permanent set of modules seen at SUPERTRAIN for a giant 16 Day Christmas Run, December 21-January 5, closed Christmas Day, Boxing Day and New Years Day. Run times are 10 am-6 pm. Visitors are welcome to run a train you received for Christmas, just bring your individual locomotive and for \$20 for a run time of 4 hours on a large Digitrax DCC layout! Operators MUST bring a Digitrax or WiFi throttle. Admission: is \$5.00 for adults, \$2.00 for kids or just come to see the large layout and the trains!!! Questions: Text Garth at 403-650-1391 or e-mail. The additional Free-Mo adds 1008 square feet to the existing 1062 square foot CMT layout.

Calgary – SUPERTRAIN 2025 - Jim Ironside, Registrar

New Location!

SUPERTRAIN is a go at our new location, the Olympic Oval at the University of Calgary campus. Dates this year are setup Friday 4 April, show Saturday – Sunday, 5-6 April. We have lots more space and there will be lots of parking. Invitations to previous exhibitors and those who have registered went out at the beginning of November and applications are coming in. If you or your club have not received your invitation, contact Registrar@supertrain.ca. If you are new to SUPERTRAIN and want to get on our list for an invitation to exhibit, go to supertrain.ca and look for the link Application Request in blue on the left hand side - the process is all on-line.

Look forward to seeing you all there.

2025 Calgary Prototype Modellers Meet – Dave Audley

It has been almost six years since the last Prototype Modellers Meet was held in Calgary. We are excited to announce that planning is well underway for the next CPM meet. Save the dates, May 9 – 10, 2025.

The theme for this Meet is Car Loads – What’s in there? How have loads changed over time? What commodities were carried and what are now carried? What forces caused the rail industry to change and how did the industry respond?

How do we model past and present rail equipment? Associated industries. What current models are available that help us to build layouts that depict our favourite industry – both past and present?

We have 2 clinic slots for Friday evening, May 9 and 4 clinic slots scheduled for May 10.

We are looking for 6 available clinicians that can answer the above challenge. We also hope that models in the display room are related to the clinics. Of course, we are also open to displaying any and all models.

The achievement Program of the 6th Division is co-sponsoring this meet as well as the Edmonton RPM in October.

If you would like to present a clinic or would like more information, please email Dave at daudley@telusplanet.net.

The Monashee Pacific Adds to the Fleet – Tim Walker

In the mid 1950's (Purchase Order documents have been lost to time), the Monashee Pacific (MONP) purchased seven new RS-3's from MLW to be used in a variety of service over the railroad. The motive power department designated one 3 unit set for ore service, one 2 unit set for a coal turn, and one 2 unit set for local work.

Fast forward to 2016 when the Edmonton Model Railroad Association purchased five Atlas RS-3's with sound to fulfill the MONP requirements. The EMRA previously bought two units, and they have been in service for many years. I thought this would be a good project for me to complete and add to the roster, so I took the five locomotives home to begin work.



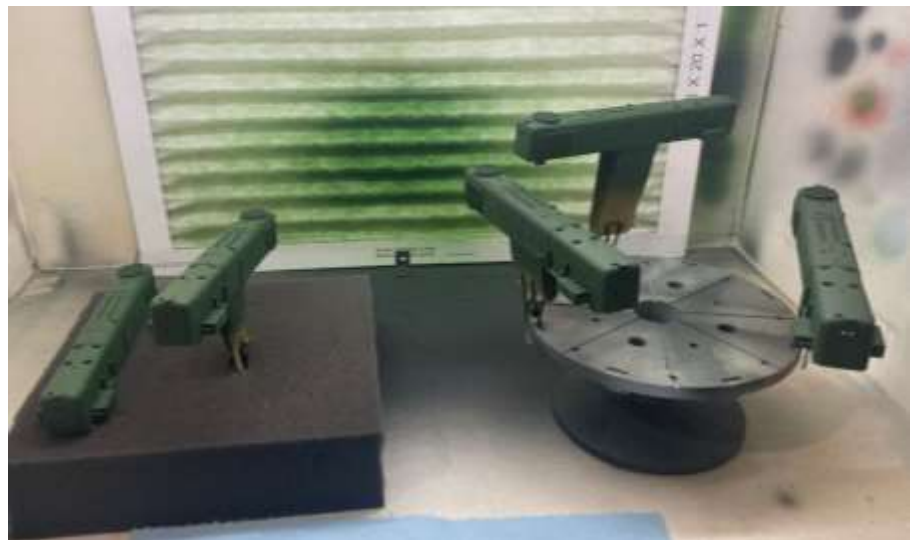
The first step was to disassemble the shells and prepare them for washing. After removing the couplers, I was able to apply light pressure to the hood for it to slide off the frame. The lights are connector to clear bars that run the length of the motor frame, so I had no wires to deal with. The cab then slid easily off the body by pulling up. The windows are a single piece of plastic and pop out with little force. Next, the walkway can be lifted off the frame, working around the motor. Finally, the fuel tank is removed by bending two tabs and it will drop out. All

parts were then washed in soapy water, rinsed, and dried.

All painting was done by using my Badger 150 airbrush in my basement. I have a large paint booth, so this allowed me to run a bit of an assembly line to complete all five units. First, everything was primed with Vallejo Black primer. I used black because I found the MONP Green came out better using black instead of grey. After everything was primed, I painted the cab and body with Vallejo Midouri Green. I was able to achieve a smooth and consistent layer with two coats. Next, the fuel tank and walkways were painted with Vallejo Black. Finally, the frame was given a yellow stripe. I was in no rush to complete this project, so each coat was given between 24-48 hours to dry.

While waiting for the main parts to dry, I painted the grab irons, brake wheel, handrails and horns. I used a piece of styrofoam to hold the grab irons, horns and brake when and applied a coat of primer, then two coats of green. The grab irons were mounted to a clothes pin for painting. After the handrails were finished, I cut them away from the molds. I found I only had to do a minor touch up with a small brush to hide the knife mark.

I sprayed the cab and body with a coat of Vallejo Gloss to prep for decals. The EMRA had ordered a decal sheet from PDC Decals with our numbers, name and a herald. The decals were applied with Micro Set, then two coats of Micro Sol to allow them to bond. Allow the decal to fully dry between all three coats.





Once the decals were set, I put the cab, hood, walkway and fuel tanks back on the frame. I sprayed the entire unit with a coat of Vallejo Matt finish to prep for weathering. Weathering was done with a mix of airbrush, and pan pastels. I weathered the units destined for coal service heavier than the others. Once weathering was complete, the locomotive received a final layer of Vallejo Matt to seal everything in.

The locomotives are getting programmed and will be in service shortly at the EMRA. Next time you are in town, stop on by, or sign up for an OOPS session and have a look at the units in action.



The Brill 55 Project – Dave Audley

It was Thursday and Dale and I were headed off to our weekly ROMEO lunch. And as usual, we stopped in at Trains & Such. We meet up with buddies, tell “War Stories” and generally look for interesting things that might end up going home.



And that indeed happened! As I walked out of the magazine “library”, I looked down at the bottom display shelf and saw an etched brass kit for a Brill 55 car! I really like these kind of kits.... but my inner voice kept saying you have enough projects to do already! I walked around the store but kept coming back to this kit! Only \$20.00! It leapt off the shelf into my hands and that was that!

Photo #1 shows the packaged kit and the instruction booklet cover. As I read the cover, I discovered the CNR was listed among the owners! An Internet search turned up some photos of CNR Brill 55 cars and I discovered that the CNR had 5 of

these plus an unusual battery car based on the Brill 55 body. This information was leading me deeper into the Brill project rabbit hole!

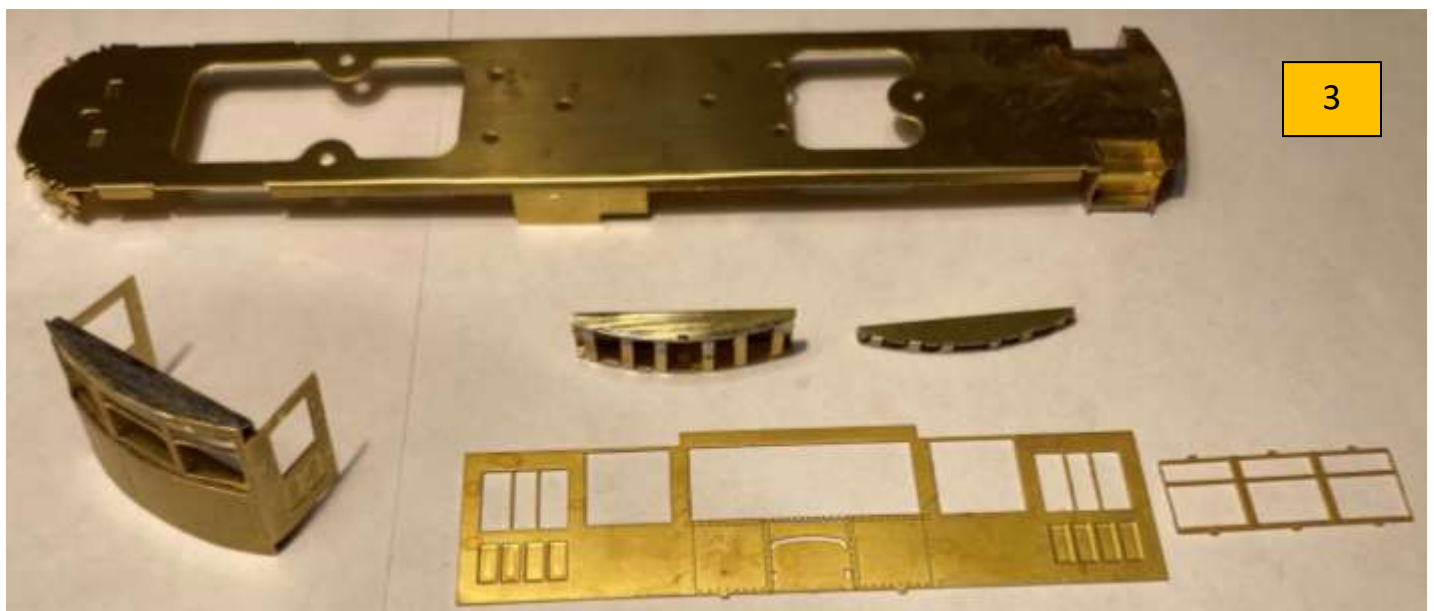
I opened the plastic bags, taking out the kit components and taking a close look at the parts. I soon realized that this was a relatively easy kit to assemble – considering all the Athabasca caboose kits I have built! Photo #2 shows all the kit parts.



The instructions indicated that ACC could be used in the assembly of this kit but I had decided that soldering the pieces together was the better way to go. I removed the floor from its fret and bent down the sides. Then I removed the steps from the third fret and bent them into the step shapes. These were then soldered to the floor.

Photo #3 shows the steps mounted to the floor. I assembled the end walls. This is the most tricky part of the assembly process. There are two pieces (one large and one small) on fret #3 that appear to look like some weird insect. These are folded per the instructions

and soldered for strength. See Photo #3. Then the two ends need to be formed. These are curved to match the curve on the “insect” pieces. I use an old Bick lighter shell to pre-form the curve. Once the curve matches the “insect” pieces, you have to bend the side window/baggage door to a 90 degree angle. Then the “insect” pieces are soldered to the formed end piece. Again, referring to Photo #3, you will see one end assembled and the parts laid out for the second end. The other tricky part is getting the window frames to fit these sub-assemblies. The windows are flat glass, so I bent the frames to a slight angle. I then filed and test fit until the frames went in as a tight fit.





I removed the sides from the #1 fret. These were soldered to one of the end sub-assemblies. See Photo #4. Take care to make sure the sides are square to the sub assembly and that the top of the sides is even with the top of the sub-assembly. The side door and window openings cover the door/window detail on the front sub assembly. Solder the rear sub-assembly to the sides. Photo #4 also shows the floor added in a test fit. There are etched brass rivet strips to be added along the top of the sides and ends. There isn't enough strip to go all the way around the top. I had to add a small strip of styrene to fill the notch showing at the front. Except for the styrene piece, these were soldered in place.

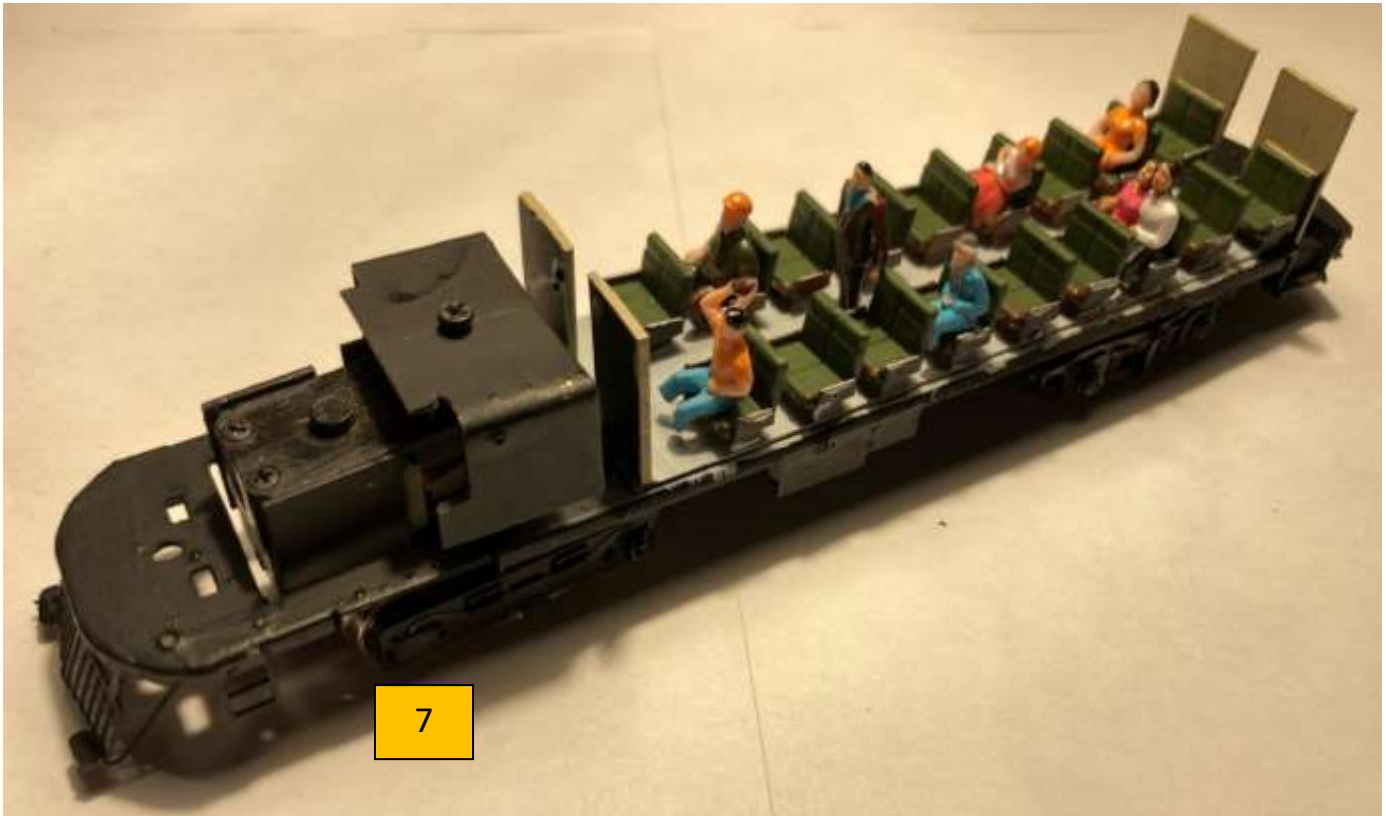


Building up the roof was next. I glued the 2 roof wood pieces together and then shaped the roof to match the curved ends. The white part showing on the front end is styrene I added as I got a little carried away sanding the roof! When I was happy with the roof fit, I added wood filler to the roof surface. This needed two applications to get rid of the wood grain. I then painted the roof with a coat of Vallejo Surface Primer. This makes any discrepancies pop right out. I then applied details as shown in Photo #6. The vents are from a Roundhouse passenger car kit. The headlight and number board are fabricated. At this point, I added the grab irons. I also soldered the "cow catcher" to the floor.



The next step was dealing with power, mounting the power and adding interior detailing. I had decided to use a self-contained power truck out of a Spectrum 44 ton diesel model. Not prototypically correct but makes this project feasible. I fabricated a bracket so that I can attach the powered truck. That is shown in Photo #7 – with the motor mounting screw in the top center. I also had to make the hole in the floor larger so that the truck could swivel. I also added under body detail – I beam frame and brake details. With the bracket soldered in place, I cleaned this assembly and painted it Scale Coat flat black. The floor had holes for the original power train (long not available). I cut a piece of styrene to fit the floor and

glued that in place. I then added end walls and seats. This was painted as shown – then passengers were added. Lots of big windows pretty much makes this mandatory! The rear truck is a freight car truck with a Loksound #50707 pick-up assembly added. This provides a more robust electrical pick up system.



The last three photos show the car being finished. The green paint is the old CN SIG CN Green #17 by Scale Coat. The decals are from the CN SIG CN Passenger Car decal set. I added lighting in the interior and the headlight using LED lighting.

And that pretty much wraps up the project – a fun build of a car and CN ownership that I hadn't known about.



DISPATCHING by SIGNAL INDICATION – Cal Sexsmith

Introduction

As technology developed and the number of trains increased it became desirable to give train authority by signal indication rather than by paper or verbal instructions. Although Centralized Traffic Control is the most well-known form of using signal indication to control trains there are others.

Rule 251

Rule 251 was mentioned in the chapter on Time Table & Train Order. This rule applies to double track only. Each track has a designated direction of travel known as current of traffic. The track is divided into blocks with signals at each block boundary. The signals can be seen only by trains travelling with the current of traffic. If the block signal is red, then the block ahead is occupied. If the block signal is yellow, the block ahead is clear but the next block is occupied. If the signal is green, then the next two blocks ahead are clear. Because all trains are travelling in the same direction most block signals are permissive. This means that a train can pass a red signal but must do so at restricted speed and be prepared to stop.

When Rule 251 is effect most trains do not have a timetable schedule. If a train has a schedule the only effect the schedule has is to prevent the train from leaving a station ahead of its schedule.

Extra trains do not require train orders, only a clearance. Once the train receives its clearance then it proceeds to its destination based on signal indication.

Train orders are still required if a train is moving against the current of traffic on the “wrong” track.

Interlocking Signals

Traditionally interlocking signals are controlled by a tower operator in an interlocking tower adjacent to the signals. The tower operator may also control switches and derails. The controls are interlocked to prevent authorizing conflicting movements.

The track and signals controlled by the interlocking tower are known as the interlocking plant. Within the plant train movements are controlled by signal indication. Red indications within interlocking plants are absolute meaning a train may not pass a red indication.

Manual Block

This system is used on busy lines which can be single, double or multiple track main lines or a combination of these. Manual block was very common in Europe and the eastern United States. In its simplest form on a single track line the track is divided into blocks with a block operator located in a block station at each block boundary. When a train approaches the block station the operator contacts the next operator to determine if the track is clear and if the train may proceed. If the train may proceed the operator then gives it authority either by manually operating a signal or by giving the train a written clearance.

In more complex situations there is an interlocking plant for each passing siding on single track and each set of crossovers on multiple tracks. Again when a train approaches the operator contacts the next operator to determine if the train can proceed and on which track. The dispatcher will be in contact with the block operators and will determine which trains have priority and may also direct which track certain trains use.

As with Rule 251 timetables only control train departure times and extra trains only require a clearance to depart. While flexible and capable of moving many trains it is very labour intensive.

Centralized Traffic Control

Centralized Traffic Control (CTC) is basically a series of interlocking plants, called control points, linked together and under direct control of one person, usually the dispatcher but a tower operator in some cases. CTC has track occupancy detection so that the location of trains can be displayed on the CTC panel and the CTC operator can set the switches and signals in an appropriate manner. Again schedules are only for the purpose of controlling departure times and a clearance is the only authorization a train requires other than signal indication. There will be automatic block signals between control points.

CTC often starts out as CTC islands with a CTC machine controlling as few as one control point but in no case covering an entire subdivision. These islands were often expanded over time to eventually become full blown CTC. It is not unusual for a train to be governed by two or more dispatching methods in the course of a run.

Some railroads also installed “poor man’s” CTC where some feature were not available. For example, the dispatcher might not have control of switches and the train crew would need to operate switches based on signal indication.



Adapting to the Model

CTC installations on model railroads are becoming more common. Programs such as JMRI allow creation of CTC panels on computer screens so that a custom built CTC panel hard wired to the layout is not necessary. Building and maintaining a panel can be a hobby in its self.

Some modellers have adopted variations on “poor man’s” CTC. A common variation is to have the dispatcher control switches and signals but with no occupancy detection. In this case the train crews or station operators have to OS train locations to the dispatcher. One well known example of this was Allen McLellan’s original Virginian & Ohio.

I am not aware of any model railroad using Rule 251 over the entire layout but Tom Enloe in the Seattle area had a short section of double track that essentially operated as Rule 251. The rest of the layout operated using TT&TO.

I have encountered manual block on some layouts that covered either all or part of the layout. David Parks’ Baltimore & Ohio in the Bay area had a multi-track mainline with four or five tower operators who controlled the switches and signals. Between the interlocking plants there were block signals in both directions and a dispatcher coordinated the operators.

Mike Rirchdorf in Chicago has a somewhat simpler system. There were a couple of major yards on the layout and the yardmaster controlled signals at the last meeting point before their yard. The signals were kept red and when a train wanted to proceed they called the yardmaster who would then set the signal to green if it was safe to proceed.

Finally, David Jasper in Minneapolis-St. Paul assigned a combination tower operator/engineer to all towns. All trains were handed off between towns. When a train was ready to depart town “A” for town “B” the operator at “A” would contact “B”. If “B” was able to take the train the operator at “B” would then run the train from “A” to “B”. Once the train was ready to leave “B” for “C” operator “B” would contact operator “C” and operator “C” would take over the train. If I recall correctly this was a DC layout not DCC.

In the next installment I will cover other methods of controlling trains.

Alberta Passenger Rail Master Plan - Dave Halliday

The Alberta government is seeking public input on its rail-passenger master plan.

The government launched a [public survey on the proposed system](#) that will be available until Dec. 20. It includes questions on costs, timelines, and potential benefits. For information, see www.alberta.ca/passenger-rail.

Regional open houses will also be held in early 2025. The government started a \$9-million feasibility study last April to assess ridership potential and determine the infrastructure needs for a provincial passenger rail system.

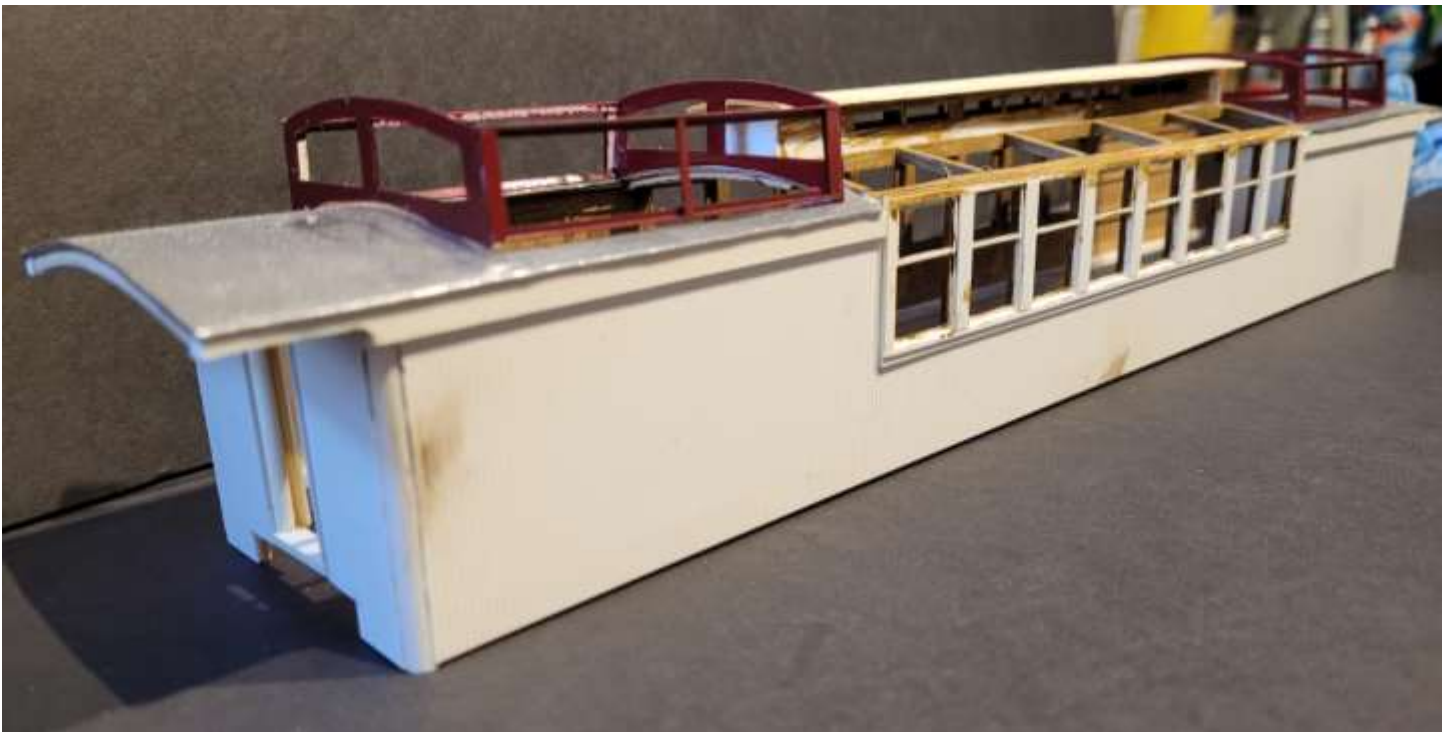
The rail master plan, to be completed by next summer, would include a high-speed rail link between Edmonton and Calgary, a rail connection between Calgary and Banff, a potential transit hub in Red Deer, rail links between Edmonton and Calgary airports and the respective downtowns as well as commuter rail service for bedroom communities such as Okotoks.

Among the questions being addressed in the online survey is what type of trains will be used, how fast they’ll go and how many stops there should be from downtown to downtown.

Scratchbuilt Models – Malcolm Anderson

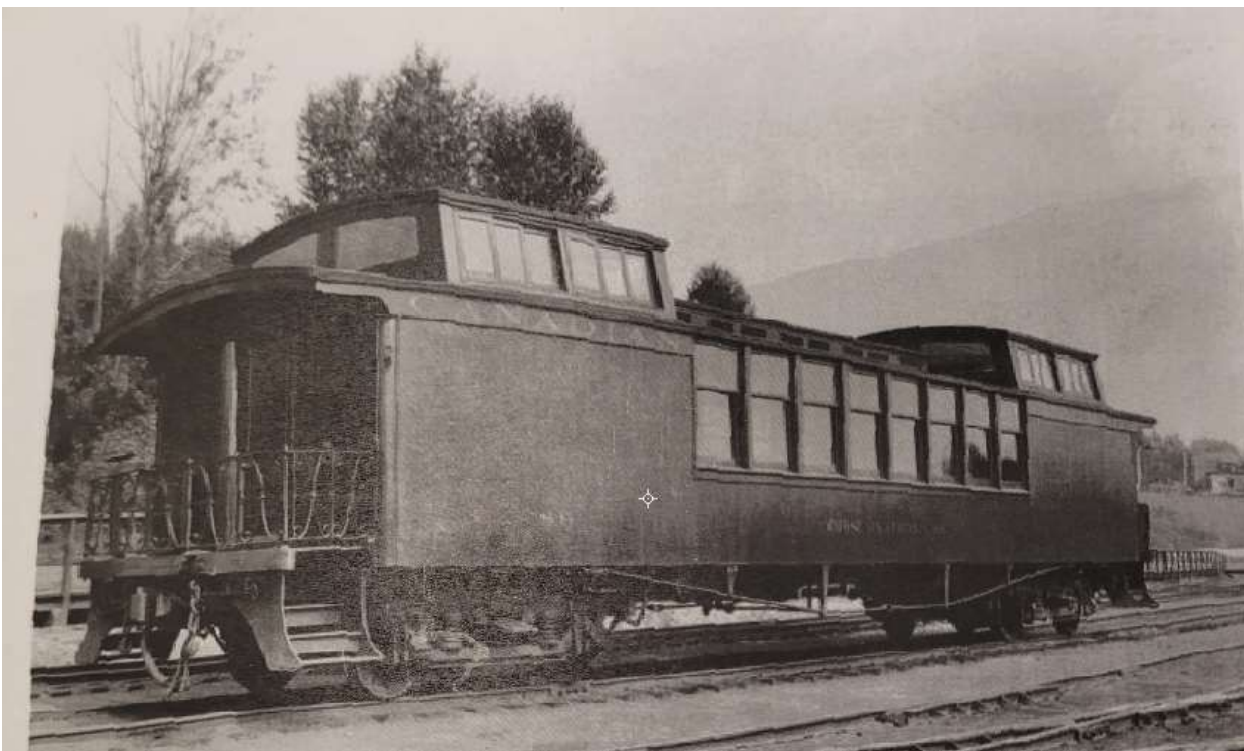


Two photos show the CP flanger 400401 created and used exclusively on the Rossland sub in southern BC. The model is proto 48 scale and is constructed of wood, styrene and brass. It has a functional blade operating system. It raises and lowers using the lever inside the cab. Must have taken the strength of Popeye to operate. I was pleased to receive an AP merit award for this car.



The next three photos show the CP Mountain Observation car under construction in O scale. Fully scratchbuilt of wood and styrene and brass. The prototype picture shows one of the very few of these cars that were built in CP shops. Not popular due to the large windows on the roof that made the car an oven. The model presents many challenges as drawings available conflict with details and measurements. The seating was made from Grandt Line parts. The cupolas

have multi-pane sliding windows. The end railings are unique and intricate and I have not decided yet on the best way to scratch build them. The roof is aluminum foil duct tape to represent the tar and fabric roof. Once primed, rough sanded and then finish painted, it will look great. Once complete, I will enter this car for AP judging as I am working on acquiring my masters on rolling stock.



Scratch building is a very rewarding part of the hobby that I enjoy. The skills learned through trial and error as well as sharing techniques with others in the hobby, help to make each new project better than the last. I will share new photos as progress moves along..

As this year 2024 gets closer to ending, we look forward to another year beginning. Alberta Midland Railway had a busy year. Santa is doing an early run at Hillcrest logging mill.



Again, the reindeer, sleigh and red suit were spotted in Camrose at Robinson's Electric for a photo op as the Santa's Christmas van is ready to be filled with presents by the generous folks of Camrose and Red Deer.



Wishing all railroaders a very Merry Christmas and a fantastic Happy New Year in 2025!
Dennis Dreher MMR

Upcoming Events in and near the 6th Division

See all the PNR events and our most recent additions at www.pnr.nmra.org

Dec 20 – Jan 5, (except Dec 24-25) Calgary, AB

Calgary Model Trainmen's Club and Calgary Free-mo present **25 Days of Christmas Run**. 10 am to 6pm daily at CMT layout, #7 – 11 St. NE, Calgary. Bring your own train to run. See: <https://www.calgarymodeltrainmen.com/>

Jan 18-19, 2025, (Sat-Sun), Prince Albert, SK

Prince Albert Model Railroaders Club's Model Trains and Hobby Show. At the Gateway Mall, show hours are 10:00 am – 5:00 pm Sat, 12:00pm – 5:00 pm Sun. For more info, please email riverxingnorth@gmail.com.

Feb 15-16, 2025 (Sat-Sun), Saskatoon SK

The Annual All Aboard Model Train Show will be held at The World Trade Centre at Prairieland Park, 803 Ruth Street E in Saskatoon. If you would like to participate or for other information please email us at allaboardtrainshow@gmail.com

Mar 2, 2025 (Sun), Calgary, AB

CMRS Spring Mini Meet and CMT Flea Market, Clinics, Show & Tell. Glenmore Inn, 9:00am - 3:00pm. For more info see: [CMRS - Events \(calgarymodelrailway.ca\)](http://CMRS-Events(calgarymodelrailway.ca))

March 22-23, 2025, (Sat-Sun), Moose Jaw, SK

Thunder Creek Model Train Show, featuring a variety of displays from across the Prairies. If you have an interest in model railroading or admire craftsmanship, you won't want to miss this amazing show. Moose Jaw Western Development Museum, 50 Diefenbaker Drive, Moose Jaw, SK

April 5-6, 2025 (Sat-Sun), Calgary, AB

SUPERTRAIN 2025, Canada's Largest Model Train Show. 9 AM to 5 PM both days. Olympic Oval at the University of Calgary campus, Calgary. Adult (17+) - \$15.00; Youth (7-16) - \$5.00; Kids (6 and under) - Free. Over 60,000 sq. ft. of operating layouts, exhibits and displays, railroad artists, hobby shops, model manufacturers, demonstrations, museums and railroad historical associations, Lego and Thomas Play Area, garden railways, ride-on trains, and much more. Info:- www.supertrain.ca or email to info@supertrain.ca

April 26-27, 2025 (Sat-Sun), Regina, SK

Regina Railfest 2025 – Model Railroad Show. Over 25,000 sq. ft. of model railroad layouts, displays, exhibits, and vendors. More information available at www.reginarailfest.com . Caledonian Curling Club, 2225 Sandra Schmirler Way.

May 9-11, 2025 (Fri-Sun), Pasco, WA

2025 PNR Convention, "The Ainsworth Connector" Pasco Red Lion Hotel and Conference Center.



Activities include self-guided layout tours, Free-mo layouts, operating sessions, clinics, Celebration Room, AP evaluations, non-rail program, Annual General Meeting, Sunday awards luncheon with keynote speaker, raffles and prizes, Modeling With the Masters (pre-convention clinic on May 8).

May 9-10, 2025, (Fri-Sat), Calgary, AB

Calgary Prototype Modellers Meet Theme Freight Car Loads, St Andrews Presbyterian Church, 703 Heritage Drive SW, Calgary, AB

May 23-25, 2025, (Fri-Sun), Burnaby, BC

Railway Modellers' Meet of British Columbia At Simon Fraser University. The emphasis is primarily on "modelling" as the name implies, with many interesting clinics, layout tours, and operating sessions. Latest information at: railwaymodellersmeetofbc.ca/

June ?-?, 2025, Hinton, AB (to be confirmed) 6th Division Spring Meet

July 14 – 19, 2025
www.nmra2025.org

Like no other
NMRA National
Convention
you have ever
attended!



Oct 25, 2025 (Sat), Northern Alberta Rail Modellers Meet (NARMM) 2025 will take place in Edmonton. Location TBA. RPM-style event welcoming to everyone. Clinics, model displays and more.
www.narmm.ca



Genesee & Wyoming Inc. celebrated the launch of its first railroad in western Canada on December 2, 2024, the 43 mile Red Deer Railway Limited (RDR).

RDR runs from Jackson to Homeglen and uses operating rights over Canadian Pacific Kansas City from Red Deer to Chigwell and to Prentiss.

A direct interchange with CPKC in Red Deer provides access to the North American rail network, while RDR's 110 car storage capacity as well as railcar-to-truck transloading and railcar switching capabilities offer broader logistics services.